

**CITY OF WINDSOR  
SCHEDULE OF MEETINGS - 2025**

JANUARY 2025						
S	M	T	W	TH	F	S
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30	31	

FEBRUARY 2025						
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MARCH 2025						
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9	✓ 10	✓ 11	✓ 12	✓ 13	✓ 14	15
16	17	18	19	20	21	22
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30	31					

APRIL 2025						
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MAY 2025						
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JUNE 2025						
S	M	T	W	TH	F	S
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JULY 2025						
S	M	T	W	TH	F	S
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20	21	22	23	24	25	26
27	28	29	30	31		

AUGUST 2025						
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24	25	26	27	28	29	30
31						

SEPTEMBER 2025						
S	M	T	W	TH	F	S
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28	29	30				

OCTOBER 2025						
S	M	T	W	TH	F	S
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26	27	28	29	30	31	

NOVEMBER 2025						
S	M	T	W	TH	F	S
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DECEMBER 2025						
S	M	T	W	TH	F	S
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21	22	23	24	25	26	27
28	29	30	31			

**CR489/2024 November 25, 2024**

- Statutory Declared Holidays - Windsor City Hall Closed
- School Holidays - March Break - March 10-14, 2025
- Regular City Council Meetings (10:00 a.m. Council Chambers, 1st floor, Windsor City Hall)
- FCM Conference - May 29 - June 1, 2025 - Ottawa, Ontario
- AMO Conference - August 17-20, 2025, Ottawa, Ontario
- Ford Fireworks - June 23, 2025

**STANDING COMMITTEES**

- Development & Heritage Standing Committee (5 members of Council + 8 citizens) 4:30 p.m.
- Community Services Standing Committee (5 members of Council) 9:00 a.m.
- Environment, Transportation & Public Safety Standing Committee (5 members of Council) 4:30 p.m.

**DEADLINES FOR SUBMISSION OF REPORTS  
SCHEDULED FOR CONSIDERATION AT  
CITY COUNCIL MEETINGS  
2025**

<b>DATE OF COUNCIL MEETING</b>	<b>DEADLINE FOR PROPOSED/DRAFT REPORTS FOR AGENDA REVIEW (Monday 12:00 p.m.)</b>	<b>DATE OF AGENDA REVIEW MEETING (Tuesday 11:00 a.m.)</b>	<b>DEADLINE FOR SIGNED REPORTS TO CAO (Wednesday 4:00 p.m.)</b>	<b>ISSUANCE OF COUNCIL AGENDA (Friday 9:00 a.m.)</b>
January 13	December 16	December 17	December 18	December 20
January 27	January 13	January 14	January 15	January 17
February 10	January 27	January 28	January 29	January 31
February 24	February 10	February 11	February 12	February 14
March 17	March 3	March 4	March 5	March 7
April 14	March 31	April 1	April 2	April 4
April 28	April 14	April 15	April 16	April 17*
May 12	April 28	April 29	April 30	May 2
May 26	May 12	May 13	May 14	May 16
June 9	May 26	May 27	May 28	May 30
July 14	June 30	July 2*	July 3*	July 4
July 28	July 14	July 15	July 16	July 18
September 8	August 25	August 26	August 27	August 29
September 22	September 8	September 9	September 10	September 12
October 20	October 6	October 7	October 8	October 10
November 10	October 27	October 28	October 29	October 31
November 24	November 10	November 12*	November 13*	November 14
December 8	November 24	November 25	November 26	November 28

**Directions:** Staff involved in drafting and/or approving Council reports are encouraged to keep this schedule handy to ensure that reports will appear on the Council meeting dates intended. Use Agenda.net to create all Council reports.

- ★ Early/late due to the holiday
- ✘ No supplementary items unless emergency.
- ✓ Each Council report must be created using Agenda.net.

**DEADLINES FOR SUBMISSION OF REPORTS  
SCHEDULED FOR CONSIDERATION AT  
IN CAMERA CITY COUNCIL MEETINGS  
2025**

<b>DATE OF IN CAMERA MEETING (if required)</b>	<b>DEADLINE FOR PROPOSED/DRAFT REPORTS FOR AGENDA REVIEW (Monday 12:00 p.m.)</b>	<b>DATE OF AGENDA REVIEW MEETING (Tuesday 11:00 a.m.)</b>	<b>DEADLINE FOR SIGNED REPORTS TO CAO (Wednesday 4:00 p.m.)</b>	<b>ISSUANCE OF COUNCIL AGENDA (Friday 9:00 a.m.)</b>
January 13	December 16	December 17	January 8	January 10
January 27	January 13	January 14	January 22	January 24
February 10	January 27	January 28	February 5	February 7
February 24	February 10	February 11	February 19	February 21
March 17	March 3	March 4	March 12	March 14
April 14	March 31	April 1	April 2	April 4
April 28	April 14	April 15	April 23	April 25
May 12	April 28	April 29	May 7	May 9
May 26	May 12	May 13	May 21	May 23
June 9	May 26	May 27	June 4	June 6
July 14	June 30	July 2 *	July 9	July 11
July 28	July 14	July 15	July 23	July 25
September 8	August 25	August 26	September 3	September 5
September 22	September 8	September 9	September 17	September 19
October 20	October 10*	October 14	October 15	October 17
November 10	October 27	October 28	November 5	November 7
November 24	November 10	November 12 *	November 19	November 21
December 8	November 24	November 25	December 3	December 5

**Directions:** Staff involved in drafting and/or approving In Camera reports are encouraged to keep this schedule handy to ensure that reports will appear on the Council meeting dates intended. Do not use Agenda.net to create In Camera reports.

★ Early/late due to the holiday

✘ No supplementary items unless emergency.

**DEADLINES FOR SUBMISSION OF REPORTS  
SCHEDULED FOR CONSIDERATION AT  
STANDING COMMITTEE MEETINGS  
2025**

DATE OF STANDING COMMITTEE MEETING	NAME OF STANDING COMMITTEE	DATE OF AGENDA REVIEW MEETING (Tuesday 11:00 a.m.)	DEADLINE FOR SIGNED REPORTS TO CAO (Wednesday 4:00 p.m.)	ISSUANCE OF STANDING COMMITTEE AGENDA
January 6	Development & Heritage	December 24	December 27*	December 27
January 29	Environment, Transportation & Public Safety	January 14	January 15	January 17
February 3	Development & Heritage	January 21	January 22	January 24
February 5	Community Services	January 21	January 22	January 24
February 26	Environment, Transportation & Public Safety	February 11	February 12	February 14
March 3	Development & Heritage	February 18	February 19	February 21
March 5	Community Services	February 18	February 19	February 21
March 26	Environment, Transportation & Public Safety	March 11	March 12	March 14
April 2	Community Services	March 18	March 19	March 21
April 7	Development & Heritage	March 25	March 26	March 28
April 30	Environment, Transportation & Public Safety	April 15	April 16	April 17*
May 5	Development & Heritage	April 22	April 23	April 25
May 7	Community Services	April 22	April 23	April 25
May 28	Environment, Transportation & Public Safety	May 13	May 14	May 16
June 2	Development & Heritage	May 20	May 21	May 23
June 4	Community Services	May 20	May 21	May 23
June 25	Environment, Transportation & Public Safety	June 10	June 11	June 13
July 2	Community Services	June 17	June 18	June 20
July 7	Development & Heritage	June 24	June 25	June 27
July 30	Environment, Transportation & Public Safety	July 15	July 16	July 18
August 5	Development & Heritage	July 22	July 23	July 25
September 2	Development & Heritage	August 19	August 20	August 22
September 3	Community Services	August 19	August 20	August 22
September 24	Environment, Transportation & Public Safety	September 9	September 10	September 12
October 1	Community Services	September 16	September 17	September 19
October 6	Development & Heritage	September 23	September 24	September 26
October 29	Environment, Transportation & Public Safety	October 14	October 15	October 17
November 3	Development & Heritage	October 21	October 22	October 24
November 5	Community Services	October 21	October 22	October 24
November 26	Environment, Transportation & Public Safety	November 12*	November 13*	November 14*
December 1	Development & Heritage	November 18	November 19	November 21
December 3	Community Services	November 18	November 19	November 21

**Directions:** Staff involved in drafting and/or approving Council reports are encouraged to keep this schedule handy to ensure that reports will appear on the intended Council/Standing Committee meeting dates. Use Agenda.net to create all standing committee reports.

**NOTE:** Development & Heritage Standing Committee reports from the meeting (first Monday of the month) will be brought to full Council at the last council meeting of the same month.

**Key:**

\* early/late due to holiday

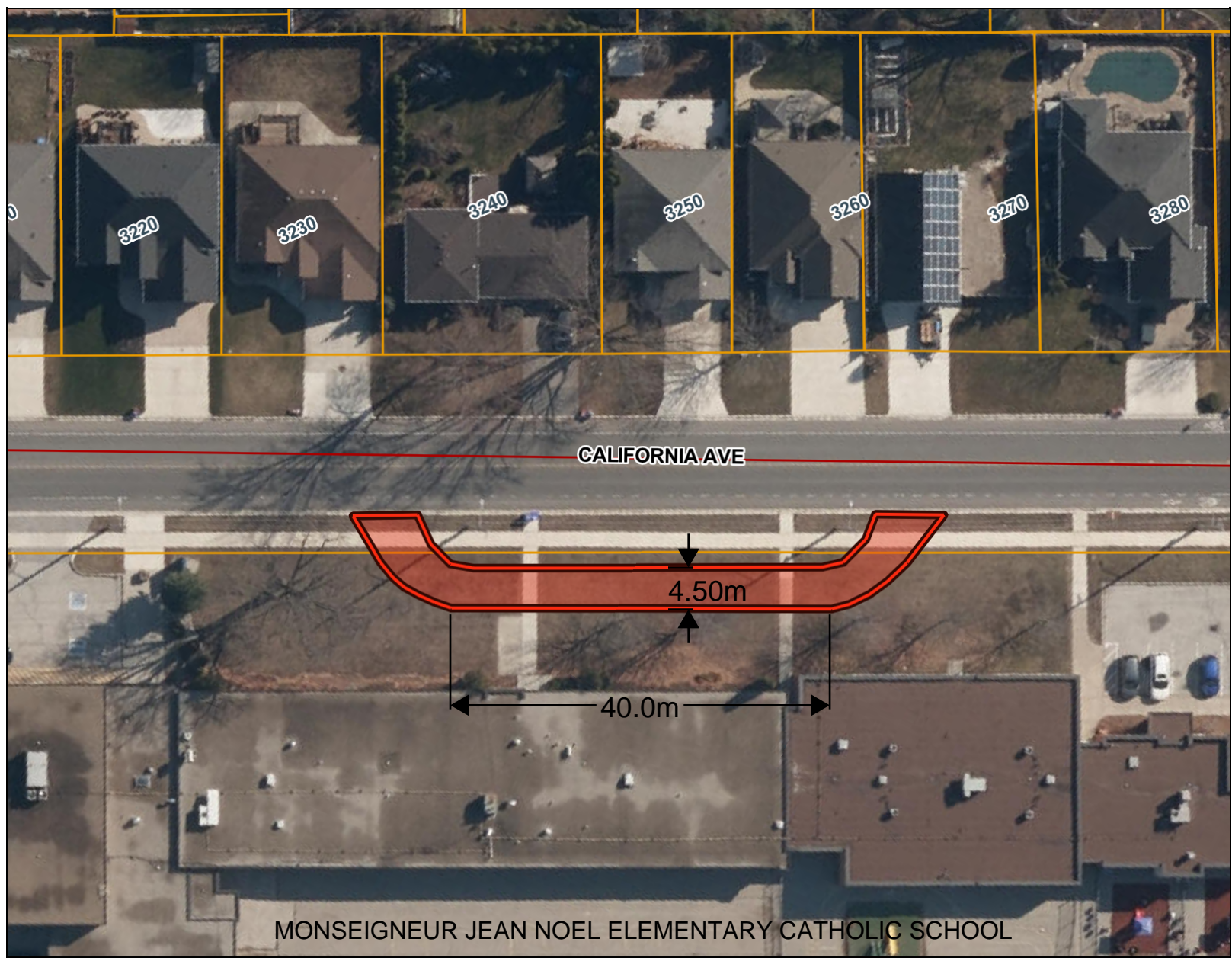
Community Services = Community Services Standing Committee

Development & Heritage = Development & Heritage Standing Committee

Environment, Transportation & Public Safety = Environment, Transportation & Public Safety Standing Committee



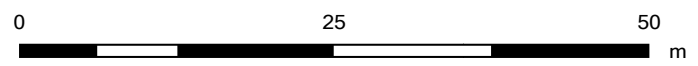
# Appendix A - Original Option



**Legend**

- Property
  - Pending Parcels
  - Parcels
- Right of Way
  - ROW Notes
- Transportation
  - Street Centreline

**Notes**

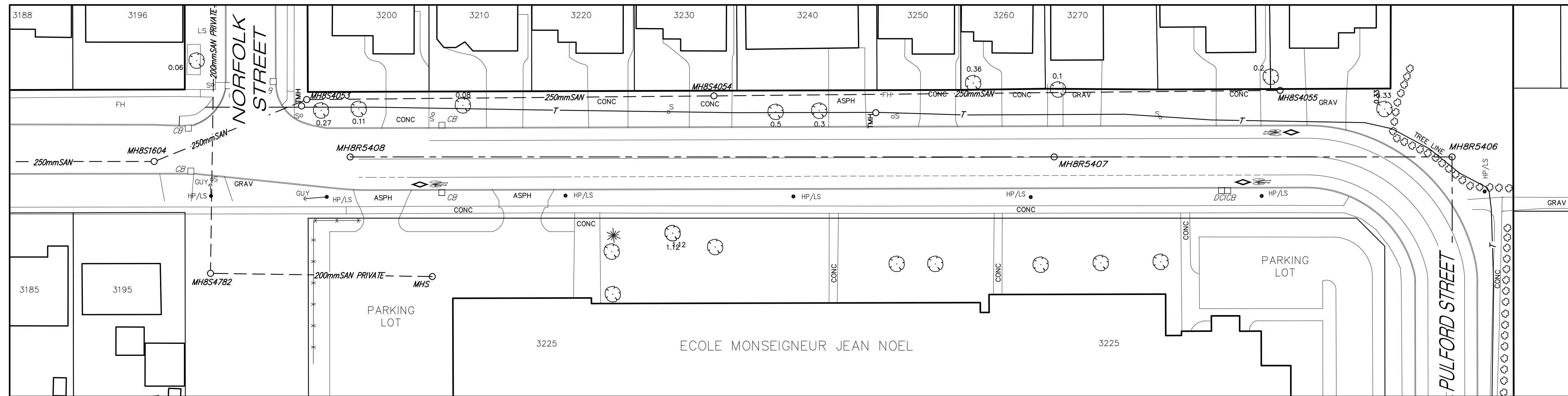
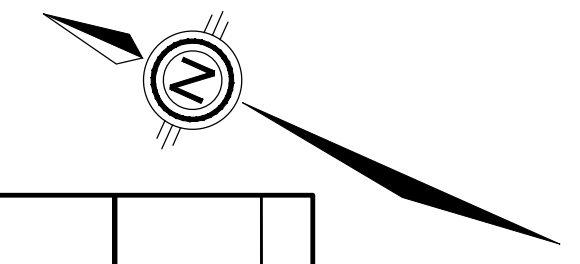


This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

THIS MAP IS NOT TO BE USED FOR NAVIGATION

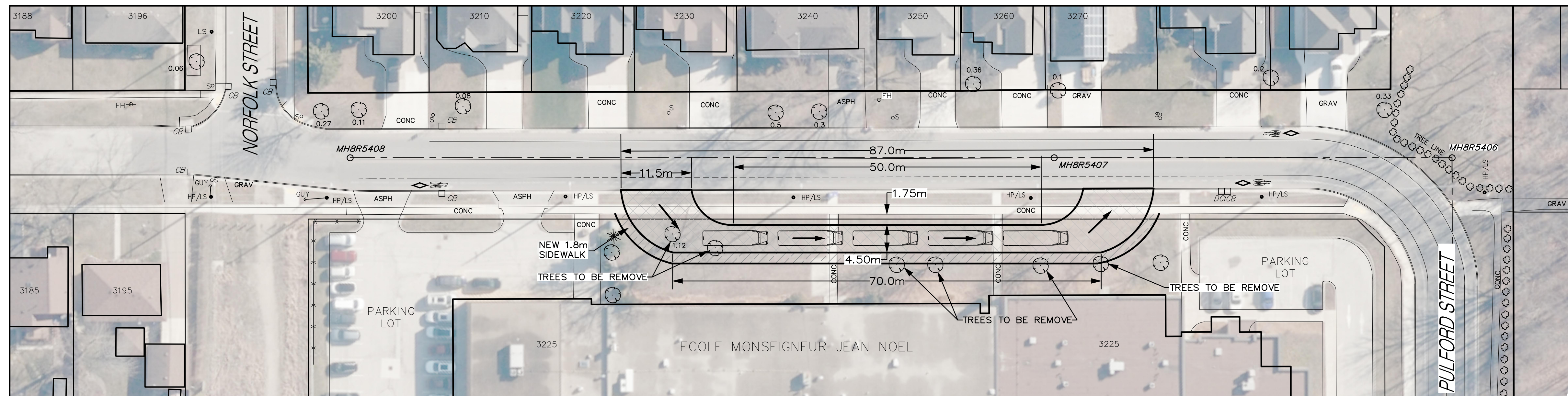
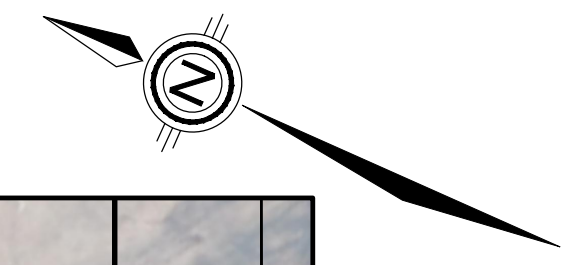
ATTENTION:  
CONTRACTOR IS RESPONSIBLE FOR THE EXACT  
LOCATION AND PROTECTION OF EXISTING  
UTILITIES DURING CONSTRUCTION.

CALIFORNIA AVENUE



EXISTING CONDITIONS & REMOVALS

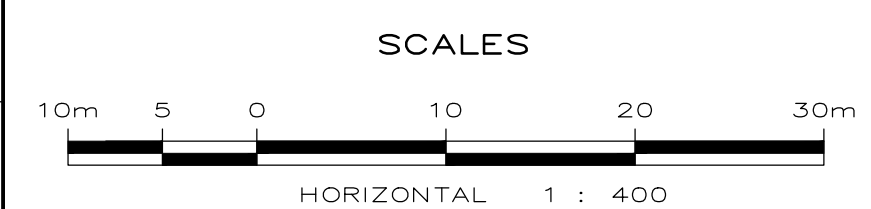
CALIFORNIA AVENUE



PROPOSED BUS BAY FOR MONSEIGNEUR-JEAN-NOEL ELEMENTARY CATHOLIC SCHOOL

Oct 25, 2024 - 3:24pm K:\Users\Etienne\Documents\Construction Drawings\2024\California\_Pulford\2024\_California\_Pulford.dwg

No.	REVISIONS	DATE	INIT.	ISSUED FOR TENDER DATE :
				ISSUED FOR CONSTRUCTION DATE :
				AS CONSTRUCTED DATE :



DATE DRAWN:  
MAY 2024  
DRAWN BY:  
E. HERNANDEZ  
CHECKED:  
P. UBENE  
DESIGN:  
P. RUGGERI  
CHECKED:  
P. MOURAD

MARK WINTERTON, P.Eng.  
CITY ENGINEER



THE  
CORPORATION  
OF THE  
CITY OF WINDSOR  
ENGINEERING DEPARTMENT

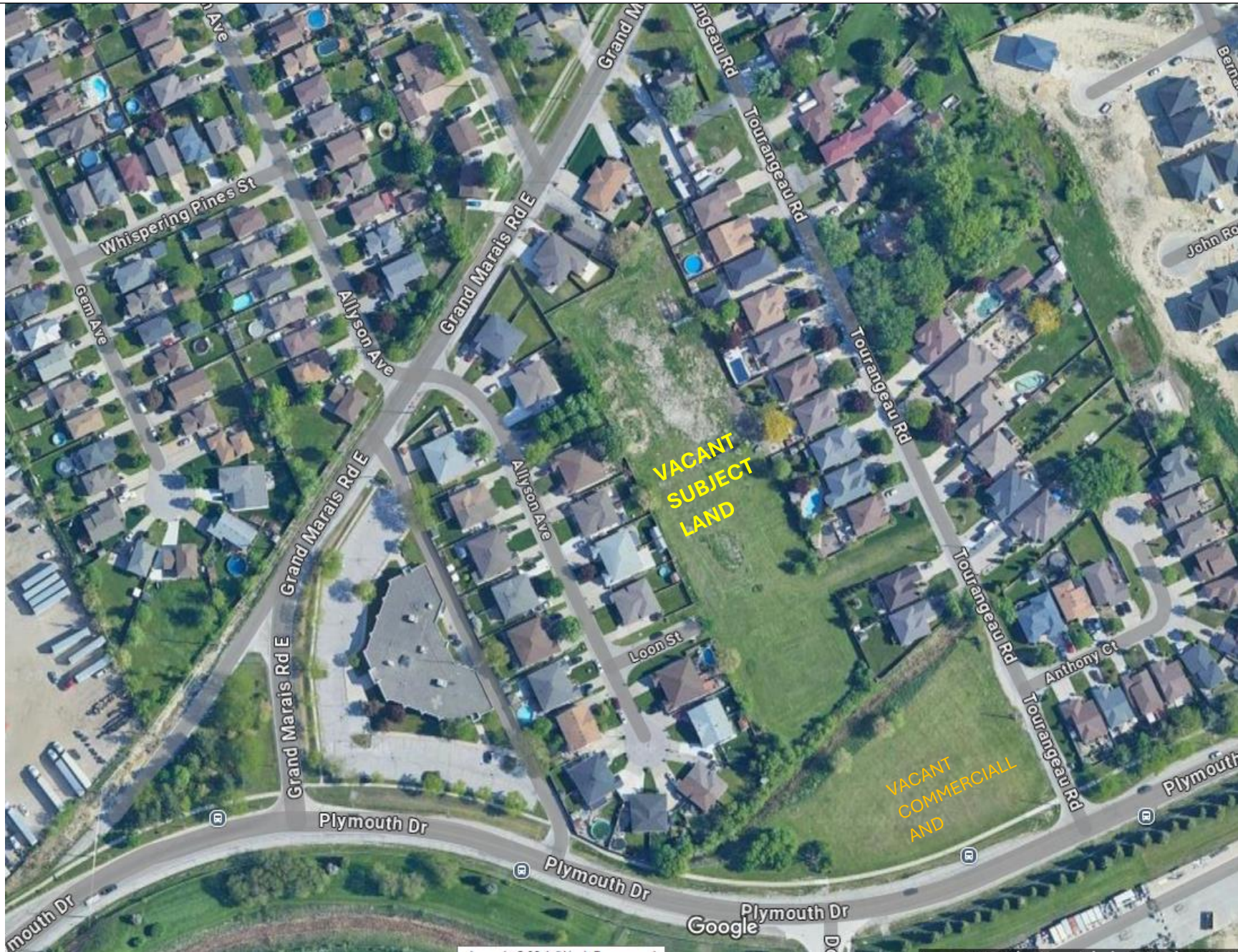
2024 BUS BAY PROGRAMME

PROPOSED BUS BAY  
CALIFORNIA AVENUE

TENDER No.	---
ACCOUNT No.	---
DRAWING No.	P-2044
SHEET	OF

APPENDIX A, SITE PHOTOS

2024 Google imagery showing the vacant subject land, the abutting vacant commercial land and surrounding uses



## APPENDIX A, SITE PHOTOS



2021 Google Photo of vacant commercial land fronting on Plymouth Drive, abutting the south limit of the subject land.



## APPENDIX A, SITE PHOTOS



2021 Google Photo showing Tourangeau Road intersection with Plymouth Drive

## APPENDIX A, SITE PHOTOS



2014 Google Photo of Loon Street intersection with Allyson Avenue



2014 Google Photo of Zelda Court intersection with Allyson Avenue

## APPENDIX A, SITE PHOTOS

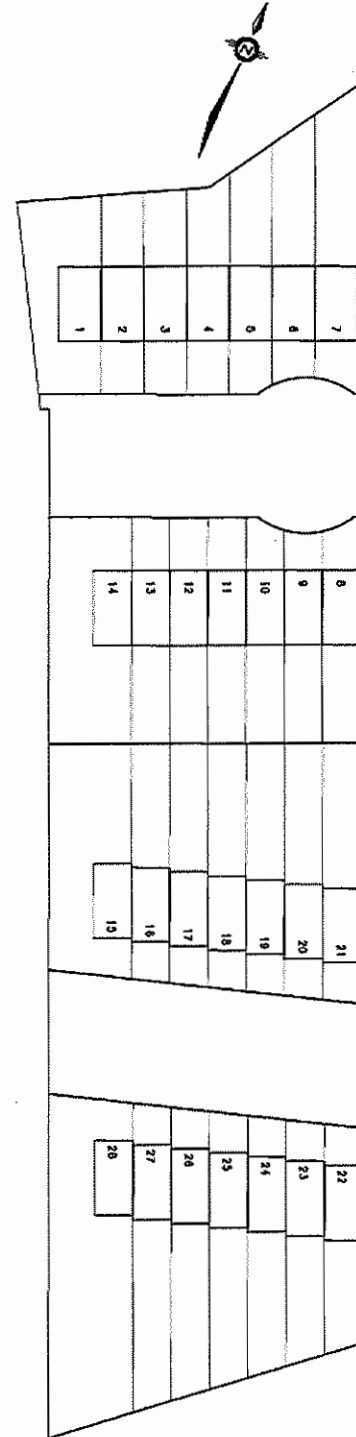


2014 Google Photo Grand Marais Road East and Allyson Avenue intersection



**APPENDIX C – DEVELOPMENT CONCEPT PLAN**

SITE DATA TABLE			
REFERENCE NUMBER	DESCRIPTION	REQUIRED	PROPOSED
	ZONING	--	RD 2.2
11.2.5.5.1	LOT FRONTAGE		
	LOTS 1–7	20 m (MIN.)	53 m
	LOTS 8–14	20 m (MIN.)	51.4 m
	LOTS 15–21	20 m (MIN.)	52 m
	LOTS 22–28	20 m (MIN.)	52.1 m
11.2.5.5.4	BUILDING HEIGHT	9.0 m (MAX.)	9.0 m
LOTS 1–7	LOT AREA 11.2.5.5.2	200 m <sup>2</sup> (MIN.) / UNIT	1,995.4 m <sup>2</sup>
	BUILDING AREA		583.4 m <sup>2</sup>
11.2.5.5.3	LOT COVERAGE	45 % (MAX.)	29.2 %
11.2.5.5.7	SIDE YARDS	1.5 m (MIN.)	1.5 / 4.1 m
LOTS 8–14	LOT AREA 11.2.5.5.2	200 m <sup>2</sup> (MIN.) / UNIT	1,855.0 m <sup>2</sup>
	BUILDING AREA		520.3 m <sup>2</sup>
11.2.5.5.3	LOT COVERAGE	45 % (MAX.)	28 %
11.2.5.5.7	SIDE YARDS	1.5 m (MIN.)	1.2 / 7.2 m
LOTS 15–21	LOT AREA 11.2.5.5.2	200 m <sup>2</sup> (MIN.) / UNIT	2,041.4 m <sup>2</sup>
	BUILDING AREA		520.3 m <sup>2</sup>
11.2.5.5.3	LOT COVERAGE	45 % (MAX.)	25.5 %
11.2.5.5.7	SIDE YARDS	1.5 m (MIN.)	1.2 / 7.2 m
LOTS 22–28	LOT AREA 11.2.5.5.2	200 m <sup>2</sup> (MIN.) / UNIT	2,335.4 m <sup>2</sup>
	BUILDING AREA		520.3 m <sup>2</sup>
11.2.5.5.3	LOT COVERAGE	45 % (MAX.)	22.3 %
11.2.5.5.7	SIDE YARDS	1.5 m (MIN.)	1.2 / 7.46 m
24.20.5.1	PARKING SPACES	1 PER UNIT W/ GARAGE	28



MAP NO : SDN-003/24-2  
 APPLICANT : OLIVIA CONSTRUCTION HOMES INC.  
 DATE: SEPTEMBER, 2024  
 CITY OF WINDSOR PLANNING DEPARTMENT

# APPENDIX D

## RESULTS OF CONSULTATION

### with Municipal Departments and External Agencies

#### **BELL CANADA – JUAN CORVALAN**

We have reviewed the circulation regarding the above noted application. The following paragraphs are to be included as a condition of approval:

#### **Bell Canada Condition(s) of Approval**

1) The Owner acknowledges and agrees to convey any easement(s) as deemed necessary by Bell Canada to service this new development. The Owner further agrees and acknowledges to convey such easements at no cost to Bell Canada.

2) The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.

Upon receipt of this comment letter, the Owner is to provide Bell Canada with servicing plans/CUP at their earliest convenience to [planninganddevelopment@bell.ca](mailto:planninganddevelopment@bell.ca) to confirm the provision of communication/telecommunication infrastructure needed to service the development.

It shall be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada's existing network infrastructure to service this development. In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development.

#### **Concluding Remarks:**

To ensure that we are able to continue to actively participate in the planning process and provide detailed provisioning comments, we note that we would be pleased to receive circulations on all applications received by the Municipality and/or recirculations.

If you believe that these comments have been sent to you in error or have questions regarding Bell's protocols for responding to municipal circulations and enquiries, please contact [planninganddevelopment@bell.ca](mailto:planninganddevelopment@bell.ca) directly.

We note that WSP operates Bell Canada's development tracking system, which includes the intake and processing of municipal circulations. **However, all responses to circulations and requests for information, such as requests for clearance, will come directly from Bell Canada, and not from WSP.** WSP is not responsible for Bell's responses and for any of the content herein.

#### **ASSET MANAGEMENT – JOSE MEJALLI**

No objection to the change from RD2.1 to RD2.2 to permit the proposed townhome dwellings on the subject land.

#### **ENBRIDGE – SANDRO AVERSA**

After reviewing the provided drawing, and consulting our mapping system, please note that Enbridge Gas has no active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

Please contact me if you have any further questions or concerns.

### **TRANSIT WINDSOR – JASON SCOTT**

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Central 3. The closest existing bus stop to this property is located on Plymouth at Tourangeau Northwest Corner. This bus stop is approximately 220 metres from this property falling within Transit Windsor's 400 metre walking distance guidelines to a bus stop. This will be maintained with Transit Windsor's City Council approved Transit Master Plan.

### **CANADA POST – BRUNO DESANDO**

Thank you for contacting Canada Post regarding plans for a new development in the City of Windsor. Please see Canada Post's feedback regarding the proposal, below.

#### **Service type and location**

1. Canada Post will provide mail delivery service to the subdivision through centralized Community Mail Boxes (CMBs).
2. If the development includes plans for (a) multi-unit building(s) with a common indoor entrance, the developer must supply, install and maintain the mail delivery equipment within these buildings to Canada Post's specifications.

#### **Municipal requirements**

1. Please update our office if the project description changes so that we may determine the impact (if any).
2. Should this subdivision application be approved, please provide notification of the new civic addresses as soon as possible.

#### **Developer timeline and installation**

1. Please provide Canada Post with the excavation date for the first foundation/first phase as well as the date development work is scheduled to begin. Finally, please provide the expected installation date(s) for the CMB(s).

**Please see Appendix A for any additional requirements for this developer.**

#### **Appendix A**

##### **Additional Developer Requirements:**

- The developer will consult with Canada Post to determine suitable permanent locations for the Community Mail Boxes. The developer will then indicate these locations on the appropriate servicing plans.

- The developer agrees, prior to offering any units for sale, to display a map on the wall of the sales office in a place readily accessible to potential homeowners that indicates the location of all Community Mail Boxes within the development, as approved by Canada Post.
  - The developer agrees to include in all offers of purchase and sale a statement which advises the purchaser that mail will be delivered via Community Mail Box. The developer also agrees to note the locations of all Community Mail Boxes within the development, and to notify affected homeowners of any established easements granted to Canada Post to permit access to the Community Mail Box.
  - The developer will provide a suitable and safe temporary site for a Community Mail Box until curbs, sidewalks and final grading are completed at the permanent Community Mail Box locations. Canada Post will provide mail delivery to new residents as soon as the homes are occupied.
  - The developer agrees to provide the following for each Community Mail Box site and to include these requirements on the appropriate servicing plans:
    - Any required walkway across the boulevard, per municipal standards
    - Any required curb depressions for wheelchair access, with an opening of at least two metres (consult Canada Post for detailed specifications)
- A Community Mailbox concrete base pad per Canada Post specifications.

## **ZONING COORDINATION – PIERE BORDEAUX**

- **Current Zoning Designation:** Residential District 2.1 [RD2.1]
- **Proposed Zoning Designation:** Residential District 2.2 [RD2.2]
- **Existing Use:**
  - Vacant
- **Proposed Use:**
  - Residential Development - 7-unit townhome dwellings of 28 total residential units
- **Section 5 – General Provisions:**
  - [COMPLY]
    - [5.40] Flood Protection & Natural Hazards Provisions
      - The lot is situated within Detroit River/Lake St-Clair flood prone area
        - An input from ERCA is required
- **Section [11.2] – [RESIDENTIAL DISTRICT 2.2] [RD2.2]:**
  - [COMPLY]
  - [11.2.5.5.1] Minimum Lot Width:
    - [20.0 m] (Required)
    - [51.0 m] (Provided)
  - - [11.2.5.5.2] Minimum Lot Area
      - [200.0 m<sup>2</sup>] (Required)
      - [264.5 m<sup>2</sup>] (Provided)
- **Section 20 – Site Specific Zoning Exemptions:**
  - [NOT APPLICABLE]



- **Section 24 – Parking, Loading, and Stacking Provisions:**
  - [APPLIES]
- **Section 25 – Parking Area Regulations:**
  - [NOT APPLICABLE]

**GREATER ESSEX COUNTY DISTRICT SCHOOL BOARD & WINDSOR ESSEX CATHOLIC DISTRICT SCHOOL BOARD – JORDAN COOK**

On behalf of the Greater Essex County District School Board and the Windsor Essex Catholic District School Board, we confirm receipt of the Zoning By-law Amendment and Draft Plan of Subdivision application dated September 10, 2024. A total of 28 residential townhouse units are proposed for this development. With respect to this application, the following comments are submitted:

Greater Essex County District School Board

Comments:

This development falls within the attendance boundaries of:

- William G. Davis Public School (K-8)
- W. F. Herman Academy Secondary (9-12)

At this time, sufficient space exists within the local elementary and secondary schools to accommodate additional students from the development as proposed.

Windsor Essex Catholic District School

Comments:

This development falls within the attendance boundaries of:

- W.J. Langlois Catholic Elementary School
- F.J. Brennan Catholic High School

Although the impact of this development will be minimal, the local elementary school is operating near capacity and may not accommodate all students from new development. Interim accommodation measures such as portables or a holding area/boundary change may be required to accommodate students from this proposed development.

At this time, sufficient space exists within the local secondary school to accommodate additional students from the development as proposed.

**General Comments:**

We will continue to monitor development growth in the City of Windsor on behalf of both the GECDSB and WECDSB as it relates to the cumulative impact on local schools. The GECDSB and WECDSB also request notification of any modifications, community consultations, appeals, or notices of decision related to this application.

Please note that further to the comments provided, the GECDSB and WECDSB reserve the right to revise their position as needed without further notice. Should you require additional information regarding these comments, please contact [vw@watsoncon.ca](mailto:vw@watsoncon.ca).

## **WINDSOR POLICE – BARRY HORROBIN**

I have reviewed the draft plan of subdivision associated with this Zoning By-law amendment to create four (4) blocks comprising a total of 28 townhome dwelling units and would advise as follows:

### **EMERGENCY VEHICULAR ACCESS**

- The Windsor Police Service has no objections to the plan/layout being proposed for this plan of subdivision. The plan as proposed will fully support and facilitate the ability of the Windsor Police Service to carry out incident response (both emergency and non-emergency in nature) and general police patrolling activities, once constructed.

### **SPECIFIC SAFETY ISSUES & CONSIDERATIONS**

The following issues are hereby raised for due consideration, with the goal being to optimize public safety in a practical manner (particularly since site plan control will not apply here):

- When responding to incidents, particularly those with high risk/priority, it is critical for police responders to accurately locate the right address where an emergency call for assistance is required. This is particularly important for a situation such as this, whereby the built form of the 28 townhomes is likely to be visually identical. Therefore, it is very important that each separate dwelling unit have a prominently displayed address number that is at least 5" high, is of a contrasting colour to the backdrop onto which it is mounted and can be easily seen from the adjacent roadway (Street 'A' and Loon Street) by police responders without obstruction. This will optimize the address identification by Police/Fire/Ambulance during an emergency response.
- Pedestrian safety is very important in all residential neighbourhoods. This includes ensuring appropriate sidewalk infrastructure is provided to keep pedestrians off the roadway and to connect to adjacent areas. Proper street lighting is very important as well. Lighting provided should be LED, in keeping with the current municipal standard, which helps in promoting public safety.
- Proper lighting is critical for ensuring public safety of each residential property and its occupants. At a minimum, the following illumination levels should be provided:

Each exterior door should have a porch light that yields, ideally, 4.0 to 4.5 foot-candles of illumination

## **ENWIN**

### **HYDRO ENGINEERING: Tia McCloskey**

No Objection

Please note the following distribution and services:

- Underground 16kV primary distribution cable going from Allyson Ave to Loon St.
- Existing underground minipad transformer, 100kVA, 120/240V on Loon St.
- Secondary underground cable servicing multiple properties on Allyson Ave.
- Overhead to underground secondary cables servicing multiple properties on Tourangeau Rd.
- Overhead secondary cables servicing multiple properties on Tourangeau Rd.
- Overhead 16kV Primary distribution pole line on Tourangeau Rd, over the entrance of Loon St.
- Existing 120/240V secondary overhead transformer bank on Tourangeau Rd, at the entrance of Loon St.

- Overhead 120/240V secondary triplex distribution pole line on Tourangeau Rd, over the entrance of Loon St.
- Underground 120v Street light duplex adjacent to Plymouth Dr.
- Overhead to underground secondary cables servicing multiple properties on Grand Marais Rd E.
- Overhead secondary cables servicing multiple properties on Grand Marais Rd E.

\*Proposed buildings and/or building additions must have adequate clearance requirements from all hydro distribution and services.

We recommend referring to the Occupational Health and Safety Act for minimum safe limits of approach during construction and the Ontario Building Code for adequate clearance requirements for New Buildings and/or Building Additions.

**WATER ENGINEERING: Bruce Ogg**

ENWIN Water has no objections.

**DEVELOPMENT ENGINEERING – JUAN PARAMO**

The Applicant is requesting draft plan of subdivision approval and zoning by-law amendments to develop a residential subdivision.

The Engineering Development Division has conducted a preliminary review of the supporting documents pertaining to the subject application and have the following comments:

**Sanitary and Storm Servicing**

A Functional Servicing Study, dated April 2024 by Haddad Morgan & Associates, has been received and reviewed. The applicant's consultant has confirmed that the existing 250mm PVC sanitary sewer on Allyson Avenue and the municipal Pillette Drain No.1 will effectively accommodate the servicing needs of the proposed residential subdivision. The study demonstrates that the municipal sanitary sewer and municipal drain has adequate capacity, and no adverse impacts are expected on the surrounding areas as a result of the proposed development. The servicing study has been deemed acceptable, and the proposed servicing strategy is supported by the Engineering Development department.

As part of detailed design, a revised servicing study must be submitted, to address minor comments that have been provided separately. The revised servicing study must be to the satisfaction of the City Engineer, and should include a detailed design of both the internal subdivision sanitary and storm sewer systems, adhering to all applicable standards.

The Pillette Drain No. 1 is a municipal drain governed by by-laws under the Drainage Act. At the applicant's request, the City has initiated the process to abandon this drain. This process is currently ongoing, and any new storm outlet to the municipal drain is subject to the completion and approval of the abandonment process.

A portion of the Pillette No.1 municipal drain currently runs through the subject property. A land conveyance of approximately 4.7 metres along the southern property boundary is required to accommodate the entire municipal drain within City-owned property. In addition, a 6-metre municipal easement is required from the new southern property line to permit maintenance of the municipal drain or, should the abandonment process is completed, municipal ditch.

The development site are within the regulated area under the jurisdiction of the Essex Region Conservation Authority (ERCA), and the drain is classified as a Class F drain by the Department of Fisheries and Oceans Canada (DFO). Any impact or alterations to the drain would be subject to permits from ERCA and the DFO, working easements for drain maintenance, and to the

provisions of the Drainage Act administered through the City of Windsor. The Developer shall obtain a permit, or clearance, from the Essex Region Conservation authority (ERCA) for the outlet(s) to drain.

### **Roads and Right-of-Way**

Street A shall be classified as a Local road in accordance with the direction of the Official Plan, requiring a right-of-way width of 20.0 meters. Furthermore, the right-of-way around the proposed permanent cul-de-sac will require a consistent boulevard width as stipulated by the City of Windsor Standard Drawing AS-206C, and a 0.3m reserve at the terminus of Street A.

The Loon Street extension shall be classified as a Local road in accordance with the direction of the Official Plan, requiring a right-of-way width of 20.0 meters.

All new roadways are to be designed to City of Windsor Standard AS-206C, the City's Standard Urban Cross Section, and the City of Windsor Development Manual. Curbs and Gutters to be constructed as per City of Windsor Standard AS-208. Pavements may be 7.4 meters in width, as shown on Standard City Drawing AS-206C. The owner further agrees that cul-de-sac pavements shall have a minimum radius of 9.5 meters

In summary, we have no objection to the proposed plan of subdivision approval and the associated zoning by-law amendment. The Engineering Department recommends the following conditions be included as requirements for the approval of both the zoning amendment and plan of subdivision:

#### **Right-of-Way:**

1. The Owner agrees to dedicate to the Corporation on the registered subdivision plan, all public highways as shown on the approved draft plan of subdivision.
2. The owner shall arrange to have all public highways named and obtain street numbers for all lots, all to the satisfaction of the Corporation.
3. The owner agrees to construct on all future municipal right-of-way, pavements, including curbs and gutters, driveway approaches and the necessary drainage facilities according to City of Windsor standard specifications, complete with a 20-meter right-of-way. Pavements may be 7.4 meters in width, as shown on Standard City Drawing AS-206C. The cul-de-sac pavements shall have a minimum radius of 9.5 meter. The owner further agrees that one (1) full winter shall elapse following the laying of base asphalt, prior to the laying of surface asphalt. All work to be to the satisfaction of the City Engineer.

**Reserves** – The owner covenants and agrees that dead-ended highways shall terminate in 0.3 metre reserve blocks. The owner further agrees to gratuitously convey to the Corporation those 0.3 metre reserves in fee simple and without encumbrance and prior to the issuance of any construction permits.

**Easements** – Prior to the issuance of a construction permit, the owner shall gratuitously grant a 6m easement along the southern property boundary for the maintenance and improvement of the Pillette No. 1 Drain. Should the municipal drain be abandoned, the easement would still be necessary for the maintenance of the municipal ditch.

**Land Conveyance** – Prior to the issuance of a construction permit, the owner(s) shall agree to gratuitously convey to the Corporation, land sufficient to fully accommodate the municipal Pillette Drain No.1 within City-owned property located southerly of the drain. This conveyance shall be approximately 4.7 meters.

**Sidewalks** -The owner(s) agrees, to construct at their expense and according to City of Windsor Standard Specifications, concrete sidewalks constructed to the satisfaction of the City Engineer. Sidewalks are to be constructed at the following locations:

- New Street A – along the north boulevard to Allyson Avenue
- Loon Street – along the north boulevard between Allyson Avenue and Tourangeau Road

**Plan of Subdivision Agreement** - The applicant enter into an agreement with the City of Windsor for all requirements under the General Provisions of the Plan of Subdivision Agreement for the Engineering Department.

**Servicing Study** – The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall review the proposed impact and recommend mitigating measures and implementation of those measures.

**Site Servicing Plans** – The owner agrees to submit a site servicing plan for the subject lands to the satisfaction of the Chief Building Official, the City Engineer, and ERCA in regulated areas, prior to the issuance of any construction permits for the subject lands.

**ERCA Requirements** – The owner(s) further agrees to follow all drainage and flood proofing recommendations of the Essex Region Conservation Authority (ERCA) may have with respect to the subject land, based on final approval by the City Engineer. If applicable, the Owner will obtain all necessary permits from ERCA with respect to the drainage works on the subject lands.

**Servicing Charges** – The owner agrees to pay owned serving fees in the amount of \$11,557.70 + HST + an annual interest charge applied based on the Infrastructure Ontario 5-year borrowing rate plus 1%.

If you have any further questions or concerns, please contact Juan Paramo, of this department at 519-255-6100, ext. 6353

### **ERCA – ALICIA GOOD**

Our office has reviewed this proposal based on the mandate of the Essex Region Conservation Authority and we provide the following comments.

The City of Windsor has received application for Draft Plan of Subdivision approval SDN-003/24 and Zoning By-law Amendment Z-018/24 for the above noted subject lands.

The applicants propose to develop a residential subdivision comprising 4 blocks for townhome dwellings with a total of 28 townhome units, one Block for 1-foot land reserve along the east limit of a proposed new R.O.W. (Street A), and another Block (part of the existing southerly open drain) to be conveyed to the city. The draft plan also shows two proposed internal rights-of-way (Loon St. extension and Street A) connecting to Allyson Avenue.

The subject land is designated Residential on Schedule D – Land Use, Official Volume 2, and zoned Residential District 2.1 (RD2.1) by Bylaw 8600.

The applicant is also requesting a change from RD2.1 to RD2.2 to permit the proposed townhome dwellings on the subject land.

### **NATURAL HAZARDS AND REGULATORY RESPONSIBILITIES UNDER THE CONSERVATION AUTHORITIES ACT, O. REG 686/21, PPS**

The following comments reflect ERCA's role in protecting people and property from the threats of natural hazards and regulating development hazards lands under Section 28 of the *Conservation Authorities Act*.

The above noted lands are subject to our Regulation under the *Conservation Authorities Act* (Ontario Regulation No. 41/24). The parcel falls within the regulated area of the Grand Marais Drain and Pillette No.1 Drain. The property owner will be required to obtain an approval from the Essex Region Conservation Authority prior to any construction or site alteration or other activities affected by Section 28 of the *Conservation Authorities Act*.

### **FINAL RECOMMENDATION**

Our office has **no objection** to Application for Zoning By-Law Z 018-24.

We request the following draft conditions for inclusion in the Notice of Decision and implementing Subdivision Agreement:

1. That the subdivision agreement, between the Owner and the Municipality, contain provisions, to the satisfaction of the Municipality and the Essex Region Conservation Authority, that stipulates, that prior to obtaining final approval, for any and all phases of the development, that the Owner will finalize an engineering analysis, to identify stormwater quality and quantity measures, as necessary to control any increase in flows in downstream watercourses, in accordance with the *Windsor-Essex Region Stormwater Management Standards Manual* and any other relevant municipal/provincial, standards or guidelines, in consultation, with the ERCA;
2. That the subdivision agreement between the Owner and the Municipality contain provisions that require that the Owner install the stormwater management measures, for each phase of the development, identified in the final engineering analysis completed, as part of the development for the site and undertake to implement the recommendations contained therein, to the satisfaction of the Municipality and the Essex Region Conservation Authority;
3. That prior to final approval, the Essex Region Conservation Authority shall require a copy of the fully executed subdivision agreement between the Owner and the Municipality, in wording acceptable to the Essex Region Conservation Authority, containing provisions to carry out the recommendations of the final plans, reports and requirements noted above and obtain the necessary ERCA approvals for each phase or phases seeking final approval;
4. That prior to undertaking construction or site alteration activities, the owner shall obtain all necessary approvals from the Essex Region Conservation Authority, in accordance with Section 28 of the *Conservation Authorities Act*, any other applicable legislation, and Conditions specified herein.
5. That the subdivision agreement between the Owner and the Municipality contain provisions, to the satisfaction of the Essex Region Conservation Authority (ERCA), that require notice in every agreement of purchase and sale for the units within Block 4, that the properties are subject to Section 28 approvals by ERCA under the *Conservation Authorities Act*. Additionally, it should specify that the rear portion of the lots with 8m plus the depth of the drain may not meet the minimum setback requirement for additional structures. The ERCA permit process will assess the feasibility of constructing buildings and structures in proximity to the regulated watercourse, Pillette Drain No. 1.

If you have any questions or require any additional information, please contact the undersigned.

## **FORESTRY – YEMI ADEYEYE**

### ***Additional Comment: October 17, 2024***

FORESTRY: Tree Replacement Valuation - Z 018-24 [ZNG-7211] & SDN 003-24 [SDN-7212] - Olivia Construction Homes Inc. | 3990 Loon St.

As of October 16, 2024, all trees identified in the tree inventory conducted by Mr. Bill Rosel on the 3990 Loon St project site have now been removed or are slated to be removed. In addition, there are 8 trees along the current right-of-way for Zelda Crt. that will be lost when that roadway is extended for the project. The City requires full Tree Replacement compensation, diameter-for-diameter, for lost tree canopy for all the trees previously identified in Mr. Rosel's inventory as well as the Zelda Crt. trees.

For the trees in the inventory at 3990 Loon St. there was a total of 285 cm of tree diameter that requires replacement with 57 new caliper trees...

Valuation \$38,700.00 (based on 2024 Rates).

For the existing 8 trees along the Zelda Crt right-of-way, there is a total of 262 cm of tree diameter that requires replacement with 53 new caliper trees...

Valuation \$35,632.00 (based on 2024 Rates).

Total Canopy replacement valuation for the 3990 Loon St. project: 110 replacement trees...

Total Tree Valuation: \$74,392.00 (based on 2024 Rates).

Please let us know if you have any further questions regarding these Tree Replacement issues. Forestry will continue to monitor the progress of this construction work.

*Gaspar Horvath*

*ISA Certified Arborist/ Municipal Specialist ON-2330AM, TRAQ, TPAQ  
Forestry Supervisor, City of Windsor*

### ***Original Comment: September 24, 2024***

There are NO City trees identified on this lot. There are an estimated 40-45 private trees across portions of this lot, primarily along the South and West boundaries. The South boundary borders a drainage/ waterway, while the West backs on to existing private residences and these trees may have been planted by those residents.

The developer is requested to provide a detailed tree inventory for all live trees over 10cm DBH on the lot. The inventory should identify ownership and provide sub-meter accuracy GPS location, species, diameter (DBH) and condition for each tree. Indication should be made for each tree if it is to be preserved & protected or removed during development. A qualified, Arborist, Urban Forester or Landscape Architect should conduct the tree survey.

Forestry will require detailed site plans that show adequate Tree Root protection zones for each individual tree if they are to be preserved on-site through the development process. Details regarding the protection area required and type of fencing for Tree Root protection zones can be provided by Forestry.

If the trees are NOT to be preserved through the development process, and a request is made that specific trees be removed, the Removal & Canopy Replacement cost will be calculated based on a Diameter-for-Diameter replacement ratio for both City owned and private trees.

Forestry requests the opportunity to review landscaping plans in order to provide comment and suggestion that would maximize future on-site Tree Canopy and Tree Canopy Resilience. One suggestion at this time would be to consider maximizing tree canopy preservation particularly to the south end of the lot.

Please let us know if you have any further questions regarding Tree Protection and Replacement issues. Forestry will continue to monitor the progress of this construction work.

### **NATURAL AREAS – KAREN ALEXANDER**

- Recommends fence along Pilette Drain be installed on the north side of the existing vegetation to preserve the existing vegetation along that drain (OP 8.5 Design with Nature objectives and policies).
- Any removal of shrubs/trees must not harm active birds nests (Migratory Birds Act)

### **TRANSPORTATION PLANNING – ELARA MEHRILLOU**

Transportation Planning Comments – Revised 10-2-2024:

- Sidewalk is required on one side of Local Residential Roads, therefore, sidewalk construction per Engineering Right-of-Way requirements is required along Zelda Crt/Street A and along Loon St.
- All parking must comply with ZBL 8600 otherwise a parking study would be required.
- Consider location of existing driveways on Tourangeau when designing proposed intersection of Loon St and Tourangeau Rd.
- Potential cut through traffic to Tourangeau Rd is a concern. Follow the City's New Neighbourhood Traffic Calming Procedure to mitigate the impact of the cut-through on Loon St.
- All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
- Cul-de sac on Zalda Cret/Street A and Loon Street extension/connection to Tourangeau Rd must meet the City Standard Engineering Drawings.
- All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

### **HERITAGE PLANNING – TRACY TANG**

Please find attached Heritage Planning comments for the rezoning and subdivision application noted. Since the 2024 WAMP has been adopted, we will require a Stage 1 AA before any land disturbances (Building Permit Stage).

**Required:** Stage 1 Archaeological Assessment

The updated archaeological potential model as per the 2024 WAMP and associated Official Plan Schedule C-1 recently adopted by City Council on July 22, 2024, indicates the subject property to be within an Archaeological Potential Zone (APZ). A Stage 1 archaeological assessment and any further recommended assessments are required to be entered into the Ontario Public Register of Archaeological Reports to the satisfaction of the City of Windsor and the Ontario Ministry of Citizenship and Multiculturalism, prior to any additional land disturbances. A final copy of these relevant archaeological reports and GIS study area must be submitted to the City of Windsor.



**APPENDIX E - Open House Summary**

**ZONING BY-LAW AMENDMENT  
AND  
DRAFT PLAN OF SUBDIVISION  
FOR PROPOSED  
RESIDENTIAL DEVELOPMENT**

**3990 Loon Street  
Windsor, Ontario**

**PUBLIC CONSULTATION SUMMARY**

**September 19, 2024**

**Prepared by:**



Tracey Pillon-Abbs, RPP  
Principal Planner  
Chatham, ON  
226-340-1232  
tracey@pillonabbs.ca  
www.pillonabbs.ca

In addition to the statutory public meeting, the *Planning Act* requires that the Applicant submit a proposed strategy for public consultation with respect to an application as part of the complete application requirements.

As part of a public consultation strategy, in addition to the statutory public meeting, an informal electronic public open house was held with area residents and property owners on Thursday, September 19, 2024, from 6:00 pm to 7:00 pm.

The open house provided members of the public with opportunities to review and comment on the proposed development.

A total of **245** owners and tenants were notified, which represents a **200 m** radius from the Site.

In addition to the applicant's representatives and City Staff, 13 residents registered for the open house.

In addition, emails and phone calls were received.

The following is a summary of the comments and questions received, along with the responses provided.

Topic Item	Comments and Questions	Response
Tenure / Ownership	Are these townhomes 'geared to income housing (low income)'?  Are they individually owned?	The units will be individually owned and sold at market value.  The sales price has yet to be determined.
Trees	All trees have now been removed.	Note.
Construction	How will the environment be controlled (health issues).  Who long will construction last?	The Draft Plan of Subdivision will have requirements for construction (ie fencing, screening, etc).  There are no construction plans at this time.
Extra Property	Is there extra property that you can sell to neighbours?	Information has been forwarded to the applicant.
Drainage	Concerns about drainage issues.  Will the Grand Marais Drain / Ditch be covered?	Servicing plans will be prepared.  Grand Marais will remain an open channel section.
Traffic	Concerns with an increase in traffic.  The local roads are too narrow.	No TIS was warranted for the proposed development.

Topic Item	Comments and Questions	Response
	<p>The snow plow drive cuts into the grass every year.</p> <p>Grand Marais is too busy now.</p> <p>Can a roadway be connected to Plymouth Drive.</p> <p>Amazon will increase traffic in the area.</p> <p>There are no curbs on the local roads now.</p> <p>There are bus stops in the area that will back up traffic.</p> <p>Children's safety is at risk.</p>	<p>The Draft Plan of Subdivision will set out any road improvement required by the developer.</p> <p>The Site does not abut Plymouth Drive in order to have access.</p>
Timing of Development	What is the timing of construction?	Unknown at this time.
Open House	Purpose of the meeting?	<p>The open house provided members of the public with opportunities to review and comment on the proposed development.</p> <p>No formal application has yet been deemed complete by the City.</p>
Parking	<p>Where will people park?</p> <p>There is not enough on street parking now?</p> <p>Move the buildings back and make longer driveways.</p> <p>What side of the street will on street parking be located.</p> <p>How much street parking is proposed, and on what side of the road?</p>	<p>Each unit will provide for its own on site parking.</p> <p>Driveway length can be reviewed.</p> <p>There are minimum rear yard setback requirements set out by the City zoning by-law.</p> <p>On street parking is regulated by the City.</p>

Topic Item	Comments and Questions	Response
	We need more parking in the area.	
Building Height	You need to respect the height of the neighbourhood.  What is the maximum height allowed?	The neighbourhood is made up of a variety of different building heights.  The proposed height complies with the City of Windsor zoning by-law (9.0 m max).
Noise	Noise will be bad.	Not a planning-related issue.
Children	There are already too many school kids and buses now.	Not a planning-related issue.
Timing	When will construction start?	Pending approval of the ZBA and Draft Plan of Subdivision
Costs	How much will the units be selling for?	This has yet to be determined.
Setbacks		
Sidewalks	What side of the road will sidewalks be built?  There are no sidewalks along Allyson.	This will be set out by the City under the development agreement.
Building Type	Not in favors of townhomes.  Semi's would be better.	The density of the development most likely will be the same.
Zoning	Will the new zoning allow apartments (say 12 units)?  Can the developer change the proposal?	No, the proposed zoning will only allow a 4 unit multiple dwelling in addition to the proposed townhomes.
Soil	The site was previously used as a tree nursery, and there may be soil contamination.  There is a sample station on the Site.	No soil report is required as part of the ZBA.
Next Steps	Will be get notice of the public meeting.  Where on the City website can we find information? How often is it updated?	The City will provide the notice of the public meeting.  Search "current development applications" for the application file once it has been deemed complete.
Parcel and Block Sizes	What is the size of the entire parcel and what size are the blocks>	The Site consists of a total area of approximately 10,599.99 m <sup>2</sup> or 1.06 ha.

Topic Item	Comments and Questions	Response
		<p>The Site is an irregularly shaped lot with an approximate frontage of 15.24 m along Loon Street.</p> <p>Block 1 – 1,995.4 m<sup>2</sup>  Block 2 – 1,855.0 m<sup>2</sup>  Block 3 – 2,041.4 m<sup>2</sup>  Block 4 – 2,335.4 m<sup>2</sup></p>

Based on the comments received, there are no proposed changes to the concept plan.

**APPENDIX F -**

Excerpts from Planning Rationale Report dated September 26, 2024

## 4.4 Supporting Studies

The following supporting studies have been completed as part of this PRR in support of the application for the zoning amendment.

### 4.4.1 Servicing

A Servicing Report was prepared by Haddad Morgan & Associates Ltd, Consulting Engineers, dated July 7, 2023, and further revised on April 5, 2024.

The purpose of the report is to provide the proposed site servicing for the Site and include the stormwater storage and servicing requirements.

The proposed development requires the installation of a storm sewer system with an outlet into the adjacent municipal drain (Pillette Drain No. 1) at the southwest corner of the Site.

The report also included protection from flooding and erosion.

It was determined that full services could be provided.

### 4.4.2 Trees

A Tree Inventory and Preservation Plan was prepared by Bezaire Partners, dated December 2022.

The purpose of the plan is to provide the location of all existing trees and vegetation.

### 4.4.3 Fish

A Fisheries Act Review was prepared by MTE Consultants Inc., dated May 14, 2024.

The purpose of the plan is to complete a fish and fish habitat self-assessment to maintain compliance with the federal Fisheries Act.

It was concluded that the proposed activities would not result in the death of fish and/or fish habitat provided that the recommended mitigation and avoidance measures.

Mitigation measures include such recommendations as timing of work, sediment and erosion control fencing, response plan, machinery cleaning and erosion control.

### 4.4.4 Habitat

A Species At Risk (SAR) preliminary screening was prepared by MTE Consultants Inc., dated December 2, 2021.

The purpose of the screening was to assess the potential for habitat.

It was concluded that the Site is unlikely to contain suitable critical habitat for any Protected Species. No floral or faunal Protected Species were identified during field investigations, and no potential suitable habitat for protected species exists within the Site.

Further, the proposed activity on the Site will not contravene the Endangered Species Act (ESA) as long as recommended best management practices and mitigation measures are followed.

The assessment was filed with the Ministry, and clearance was received.

# 5.0 PLANNING ANALYSIS

## 5.1 Policy and Regulatory Overview

### 5.1.1 Provincial Planning Statement

The Provincial Planning Statement, 2024 (PPS) provides policy direction on matters of provincial interest related to land use planning and development.

The PPS was issued under Section 3 of the *Planning Act* and came into effect on October 20, 2024.

The following provides a summary of the key policy considerations of the PPS as it relates to the proposed development.

PPS Policy #	Policy	Response
Chapter 1 - Vision	Ontario will increase the supply and mix of housing options, addressing the full range of housing affordability needs. Every community will build homes that respond to changing market needs and local demand. Providing a sufficient supply with the necessary mix of housing options will support a diverse and growing population and workforce, now and for many years to come.	The proposed development provides more housing.
Chapter 2.1.4 – Buildings Homes, Sustaining Strong and Competitive Communities	To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall: a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated	The proposed development will help provide for a mix of housing options and densities to meet the needs of the City.  Full municipal services are available.



PPS Policy #	Policy	Response
	<p>and available for residential development; and</p> <p>b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.</p>	
2.1.6	<p>Planning authorities should support the achievement of complete communities by:</p> <p>a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, longterm care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;</p> <p>b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society;</p>	<p>The proposed development is consistent with the policy to achieve complete communities.</p> <p>The proposed development will provide for a range and mix of housing options.</p> <p>The Site has access to transportation options, public service facilities, other institutional uses, and parks.</p> <p>Accessibility will be addressed at the time of a building permit.</p>
2.2.1 - Housing	<p>Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:</p> <p>a) establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate income</p>	<p>The proposed development is a new housing option for the area.</p> <p>The proposed development support the City's targets.</p> <p>The needs of the residents can be accommodated as the Site is located near local amenities.</p>

PPS Policy #	Policy	Response
	<p>households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs;</p> <p>b) permitting and facilitating:</p> <ol style="list-style-type: none"> <li>1. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and</li> <li>2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;</li> </ol> <p>c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and</p> <p>d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to</p>	<p>The Site offers an opportunity for intensification and infilling.</p> <p>The proposed development will connect to existing neighbourhoods.</p> <p>The proposed density is appropriate for the Site.</p> <p>Residents will have access to nearby transit.</p>

PPS Policy #	Policy	Response
	transit, including corridors and stations.	
2.31.1 – Settlement Area	Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.	The Site is located in an existing settlement area of the City of Windsor.
2.3.1.2	Land use patterns within settlement areas should be based on densities and a mix of land uses which: a) efficiently use land and resources; b) optimize existing and planned infrastructure and public service facilities; c) support active transportation; d) are transit-supportive, as appropriate	<p>The total density of the proposed development is considered appropriate.</p> <p>The Site offers an opportunity for infilling by creating new residential dwelling lots on a vacant parcel of land.</p> <p>The proposed height and massing of the dwellings will blend with the existing residential uses in the area.</p> <p>Residents will have immediate access to shopping, employment, transit, active transportation, recreational areas and institutional uses.</p> <p>Transit is available for the area.</p> <p>Active transportation is available in the area, along Plymouth Dr.</p>
2.3.1.3	Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and	<p>The proposed development provides an infill opportunity of a vacant parcel of land.</p> <p>The Site was always intended for development.</p>

PPS Policy #	Policy	Response
	investment in the necessary infrastructure and public service facilities.	The design of the proposed development has provided a compact form.
2.3.1.4	Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.	The City has established targets. The proposed development will assist in meeting those targets as the Site is located in an existing built-up area and will add new residential housing.
2.3.1.6	Planning authorities should establish and implement phasing policies, where appropriate, to ensure that development within designated growth areas is orderly and aligns with the timely provision of the infrastructure and public service facilities.	The Site has access to existing infrastructure and nearby public service facilities.
Chapter 3.1.1 – Infrastructure and Facilities	Infrastructure and public service facilities shall be provided in an efficient manner while accommodating projected needs.	The proposed development has access to full municipal services.  There are nearby public service facilities.
3.3.3 - Transportation	Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified.	The proposed development will not have a negative impact on nearby transportation and infrastructure corridors.
3.6.2	Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized	The proposed development will be serviced by municipal sewer, water and storm, which is the preferred form of servicing for settlement areas.  A Servicing Report has been provided and summarized in Section 4.4.1 of this PRR.

PPS Policy #	Policy	Response
	servicing systems and decentralized servicing systems.	
4.1.1 – Natural Heritage	Natural features and areas shall be protected for the long term.	There are no natural heritage features that impact the Site.  Tree, Fish and Habitat reports have been prepared and summarized in Section 4.4.2 of this PRR.
4.2 - Water	Planning authorities shall protect, improve or restore the quality and quantity of water by: b) minimizing potential negative impacts, including cross-jurisdictional and cross-watershed impacts;	Permits will be obtained by ERCA in order to address the regulated area.  Support studies have been provided to address the floodplain including the Servicing Report summarized in section 4.4.1 of this PRR
Chapter 5.1.1 – Protecting Public Health and Safety	Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	There are no natural or human-made hazards that apply to this Site.  There is no risk to the public.  The Site is within the ERCA regulated area, and permits will be obtained if required.

Therefore, the proposed development is consistent with the PPS.

### 5.1.2 Official Plan

The City of Windsor Official Plan (OP) was adopted by Council on October 25, 1999, approved in part by the Ministry of Municipal Affairs and Housing (MMAH) on March 28, 2000, and the remainder approved by the Ontario Municipal Board (OMB) on November 1, 2002. Office consolidation version is dated September 7, 2012.

The OP implements the PPS and establishes a policy framework to guide land use planning decisions related to development and the provision of infrastructure and community services throughout the City.

The lands are designated "Residential" according to Schedule "D – "Land Use" attached to the OP for the City of Windsor (see Figure 4 –OP).

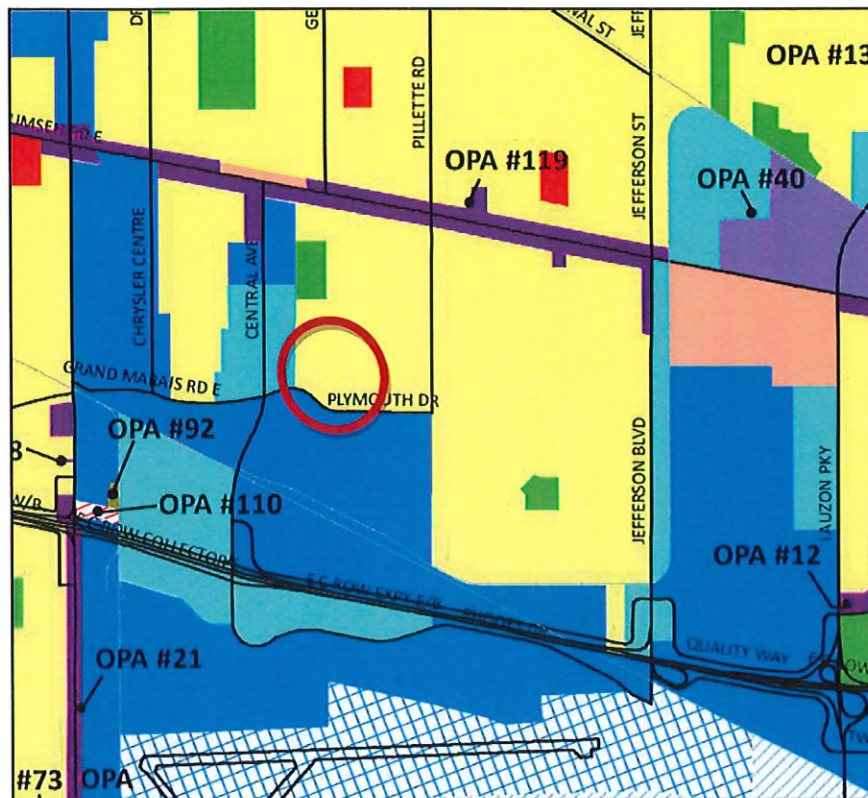


Figure 4 –OP

The following provides a summary of the key policy considerations of the OP as it relates to the proposed development.

OP Policy #	Policy	Response
3.2.1.2 – Housing Variety	Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives.	The proposed residential development supports one of the City's overall development strategies of providing for a range of housing types.  The proposed development is close to transit and local amenities.
3.3.3 - Neighbourhoods	Neighbourhoods are the most basic component of Windsor's urban structure and occupy the greatest proportion of the	The proposed residential development is in an existing built-up area of the City.

OP Policy #	Policy	Response
	<p>City. Neighbourhoods are stable, low-to-medium-density residential areas and are comprised of local streets, parks, open spaces, schools, minor institutions and neighbourhood and convenience scale retail services.</p> <p>The three dominant types of dwellings in Windsor's neighbourhoods are single detached, semi-detached and townhouses.</p> <p>The density range for Windsor's neighbourhoods is between 20 to 35 units per net hectare.</p> <p>This density range provides for low and some medium-density intensification to occur in existing neighbourhoods. Multiple dwelling buildings with medium and high-densities are encouraged at nodes identified in the Urban Structure Plan.</p>	<p>The proposed lots will blend with the surrounding area and will connect existing neighbourhoods.</p> <p>The Site offers appropriate infilling within the existing settlement area.</p> <p>The area has access to nearby amenities, such as public spaces, commercial nodes, and recreational activities.</p> <p>The proposed density offers an opportunity to efficiently use municipal infrastructure.</p> <p>The Site will provide for more housing in an existing built-up area.</p>
4.0 – Healthy Community	<p>The implementing healthy community policies are interwoven throughout the remainder of the Plan, particularly within the Environment, Land Use, Infrastructure and Urban Design chapters, to ensure their consideration and application as a part of the planning process.</p>	<p>The proposed development will support the City's goal of promoting a healthy community (live, work and play).</p> <p>The proposed development is close to nearby transit, employment, shopping, local/regional amenities, and parks.</p>
5.0 - Environment	<p>A healthy and sustainable environment represents a balance between human activities and natural features</p>	<p>The proposed development will support the City's goal of a healthy and sustainable environment.</p>

OP Policy #	Policy	Response
	<p>and functions. In order to attain this balance, Council will enhance the quality of Windsor's natural environment and manage development in a manner that recognizes the environment as the basis of a safe, caring and diverse community and a vibrant economy.</p>	<p>The Site is flat, which is conducive to easy vehicular movements.</p> <p>The Site will include new sidewalks.</p> <p>There are no anticipated traffic concerns, no environmental concerns, and no expected hazards.</p>
<p>5.4.6.4 – Development Criteria (Floodplain)</p>	<p>Council may permit development in a <b>floodplain</b> in recognized flood fringe areas outside of the floodway, including behind flood control dykes (so as to address the matter of the potential failure of protective works) provided: (a) sufficient information accompanies the application to show that the proposed development and its occupants will be protected from the effects of a Regulatory Flood; (b) the potential upstream and downstream impacts of the development proposal will not significantly affect the hydrology or hydraulics of the floodplain; (c) and that adequate floodproofing measures, determined in consultation with the Essex Region Conservation Authority, are incorporated in the development.</p>	<p>The Site is subject to ERCA regulations.</p> <p>Permits will be obtained if required prior to any building construction.</p> <p>A permit has been obtained for the placement and grading of fill for earthwork modifications.</p> <p>Support studies have been provided to address the floodplain including the Servicing Report summarized in section 4.4.1 of this PRR.</p>
<p>6.0 - Preamble</p>	<p>A healthy and livable city is one in which people can enjoy a vibrant economy and a sustainable healthy environment in safe, caring and diverse neighbourhoods.</p>	<p>The proposed development supports the policy set out in the OP as it is suited for the residential needs of the City.</p>



OP Policy #	Policy	Response
	<p>In order to ensure that Windsor is such a city, Council will manage development through an approach which balances environmental, social and economic considerations.</p>	<p>The Site will provide for more housing and will connect existing neighbourhoods.</p>
6.1 - Goals	<p>In keeping with the Strategic Directions, Council's land use goals are to achieve:</p> <p>6.1.1 Safe, caring and diverse neighbourhoods.</p> <p>6.1.3 Housing suited to the needs of Windsor's residents.</p> <p>6.1.10 Pedestrian oriented clusters of residential, commercial, employment and institutional uses.</p>	<p>The proposed development supports the goals set out in the OP as it provides for housing that is suited to residents in this area of Windsor and is close to employment and schooling opportunities.</p>
6.2.1.2 – Types of Development Profile	<p>For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan:</p> <p>(a) <b>Low Profile developments are buildings or structures generally no greater than three (3) storeys in height;</b></p> <p>(b) <b>Medium Profile developments are buildings or structures generally no greater than six (6) storeys in height; and</b></p> <p>(c) <b>High Profile developments are buildings or structures generally no greater than fourteen (14) storeys in height.</b></p>	<p>The proposed development of the lots is considered low-profile building.</p>

OP Policy #	Policy	Response
6.3.1.1 - Residential	To support a complementary range of housing forms and tenures in all neighbourhoods.	<p>The proposed development provides for a new housing choice in an existing built-up area.</p> <p>The proposed development will blend with the built form of the neighbourhood.</p>
6.3.1.2	To promote compact neighbourhoods which encourage a balanced transportation system.	The existing neighbourhood has access to transportation options, including transit.
6.3.1.3 – Intensification Infill & Redevelopment	To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan.	The Site provides an opportunity for infilling within an existing built-up area and will connect existing neighbourhoods.
6.3.2.1 – Permitted Uses	Uses permitted in the Residential land use designation identified on Schedule D: Land Use include <b>Low Profile</b> , and Medium Profile dwelling units. High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors.	It is proposed to construct townhomes, which are a permitted low profile use.
6.3.2.3 – Type of Low Profile Housing	For the purposes of this Plan, <b>Low Profile</b> housing development is further classified as follows: (a) small scale forms: single detached, semi-detached, duplex <b>and row</b> and multiplexes with up to 8 units; and (b) large scale forms: buildings with more than 8 units.	The proposed development is considered small scale.
6.3.2.4 - Location	Residential intensification shall be directed to the Mixed Use Nodes and areas in proximity to those Nodes. Within these areas Medium	<p>The Site has access to major transportation networks.</p> <p>Full municipal services are available.</p>

OP Policy #	Policy	Response
	<p>Profile buildings, up to four (4) storeys in height shall be permitted. These taller buildings shall be designed to provide a transition in height and massing from low-profile areas. New residential development and intensification shall be located where: (a) There is access to a collector or arterial road; (b) Full municipal physical services can be provided; (c) Adequate community services and open spaces are available or are planned; and (d) Public transportation service can be provided.</p>	<p>Access to transit is available with bus stops nearby.</p>
6.3.2.5 - Evaluation	<p>At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is: (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:</p> <p>(i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan; (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust; (iii) within a site of potential or known contamination; (iv) where traffic generation and distribution is a provincial or municipal concern; (v) and adjacent to heritage</p>	<p>This PRR has addressed provincial legislation in Section 5.1.1.</p> <p>The main constraint that impacts this Site is that it is located within the limit of the regulated area of ERCA. The required permits will be obtained.</p> <p>There are no nuisances.</p> <p>There is no contamination.</p> <p>Traffic generated from the proposed development is not expected to have any negative impact.</p> <p>There are no heritage resources.</p> <p>The proposed development is in keeping with policies and plans.</p>

OP Policy #	Policy	Response
	<p>resources. (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area; (c) In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas. In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets; (d) provided with adequate off street parking; (e) capable of being provided with full municipal physical services and emergency services; and (f) facilitation a gradual transition from Low Profile residential development to Medium and/or High Profile development and vice versa, where appropriate, in accordance with Design Guidelines approved by Council.</p>	<p>There are no secondary plans that impact the Site.</p> <p>The proposed development is compatible with the area as it is in keeping with the character of the existing neighbourhood.</p> <p>The proposed height of the buildings will be kept low profile, similar to the existing dwellings in the area.</p> <p>The massing of the development will fit well on the size of the lot.</p> <p>The Site is not in a mature neighbourhood as shown on Schedule A-1 of the OP.</p> <p>Off street parking is provided in private driveways.</p> <p>Full municipal services are available.</p> <p>The proposed development will be professionally designed.</p> <p>The City of Windsor intensification guidelines will be followed.</p> <p>All RD2.2 zone provisions shall comply.</p>
7.0 - Infrastructure	<p>The provision of proper infrastructure provides a safe, healthy and efficient living environment. In order to accommodate transportation and physical service needs in Windsor, Council is committed</p>	<p>The Site is close to nearby transit, has access to major transportation networks and can be serviced with full municipal services.</p>

OP Policy #	Policy	Response
	to ensuring that infrastructure is provided in a sustainable, orderly and coordinated fashion.	There will be no negative impacts on the municipal system, and it will not add to the capacity in a significant way.

Therefore, the proposed development conforms to the City of Windsor OP, and no amendment is required.

### 5.1.3 Zoning By-law

The City of Windsor Zoning By-law (ZBL) #8600 was passed by Council on July 8, 2002, and then a further Ontario Municipal Board (OMB) decision was issued on January 14, 2003.

A ZBL implements the PPS and the City OP by regulating the specific use of property and providing for its day-to-day administration.

The Site is currently zoned "Residential District 2.1 (RD2.1)" on Map 11 of the City of Windsor Zoning By-Law 8600 (see Figure 5 – ZBL).

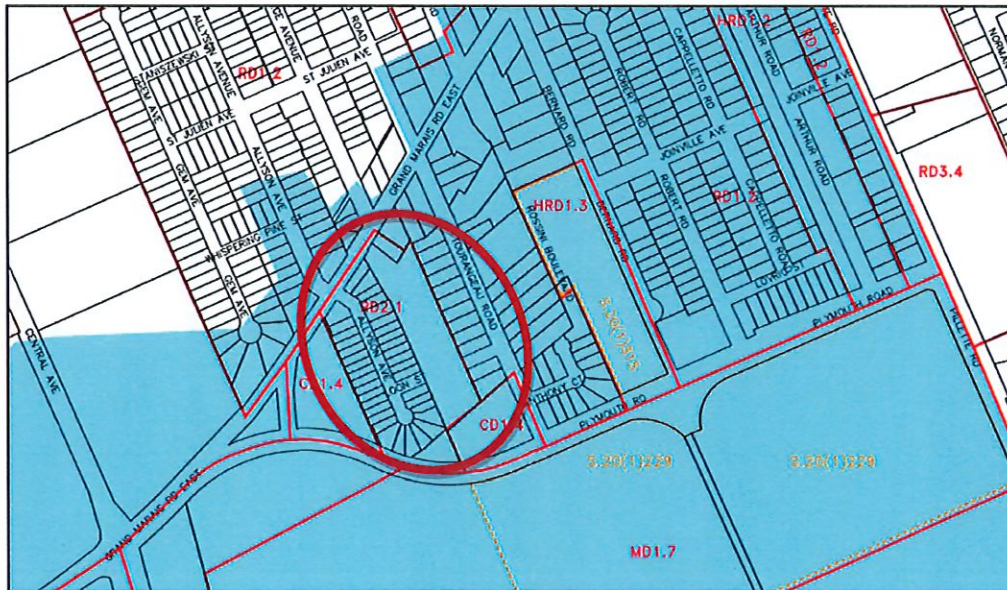


Figure 5 – ZBL

It is proposed to change the zoning to "Residential District 2.2 (RD2.2)" to permit the proposed townhome dwellings.

TOWNHOME DWELLING means *one dwelling vertically divided into a row of three or more dwelling units attached by common interior walls, each wall having a minimum area above grade of 10.0 sq. m., and may include, where permitted by Section 5.99.80, additional dwelling units. A semi-detached dwelling is not a townhome dwelling.*

A review of the RD2.2 zone provisions, as set out in Sections 11.2 of the ZBL, are as follows:

Zone Regulations	Required RD2.2	Proposed (Townhome Dwelling)	Compliance and/or Relief Requested with Justification
Permitted Uses	One Double Duplex Dwelling One Duplex Dwelling One Multiple Dwelling containing a maximum of four dwelling units One Semi-Detached Dwelling One Single Unit Dwelling <b>Townhome Dwelling</b> Any use accessory to any of the preceding uses	<b>Townhome Dwelling</b>	Complies, subject to the ZBA.  The proposed use is considered low profile and is compatible with the area.
Minimum Lot Width	20.0 m	Block 1 – 53 m Block 2 – 51.4 m Block 3 – 52 m Block 4 - 52.1 m	Complies
Minimum Lot Area	Per dwelling unit - 200.0 m <sup>2</sup>  200.0 x 7 = 1,400 m <sup>2</sup>	Block 1 – 1,995.4 m <sup>2</sup> Block 2 – 1,855.0 m <sup>2</sup> Block 3 – 2,041.4 m <sup>2</sup> Block 4 – 2,335.4 m <sup>2</sup>	Complies  As shown on the concept plan.
Maximum Lot Coverage	45.0%	Block 1 – 29.2 % Block 2 – 28 % Block 3 – 25.5 % Block 4 - 22.3 %	Complies

<b>Zone Regulations</b>	<b>Required RD2.2</b>	<b>Proposed (Townhome Dwelling)</b>	<b>Compliance and/or Relief Requested with Justification</b>
Maximum Main Building Height	9.0 m	9.0 m	Complies
Minimum Front Yard Depth	6.0 m	Shall comply	Complies
Minimum Rear Yard Depth	7.50 m	Shall comply	Complies
Minimum Side Yard Width	1.20 m	Block 1 – 1.5/4.1 m Block 2 – 1.2/7.2 m Block 3 – 1.2/7.2 m Block 4 - 1.2/7.46 m	Complies
Parking Requirements 24.20.5.1	Townhome Dwelling having an attached garage or carport - 1 for each dwelling unit	2, includes garages and driveways  (28 parking spaces in total)	Complies

Therefore, all RD2.2 zone provisions shall comply, and no relief is required.

## **6.0 SUMMARY AND CONCLUSION**

### **6.1 Context and Site Suitability Summary**

#### **6.1.1 Site Suitability**

The Site is ideally suited for residential development for the following reasons:

- The land area is sufficient to accommodate the proposed development,
- The Site is flat, which is conducive to easy vehicular movement,
- The Site has access to full municipal water, storm and sewer systems,
- There are no anticipated traffic concerns,
- There are no environmental concerns, and
- There are no hazards.

#### **6.1.2 Compatibility of Design**

The Site is compatible with the surrounding area in terms of scale, massing, height and siting.

The development pattern is proposed to be an efficient use of the Site and will connect existing neighbourhoods.

#### **6.1.3 Good Planning**

The proposal represents good planning as it addresses the need for the City to provide housing.

The proposed lots will contribute toward infilling requirements.

Residential use on the Site represents an efficient development pattern that optimizes the use of land in an existing built-up area that has residential uses surrounding the Site.

The additional residential lots will not put any additional stress on municipal infrastructure or the current Site.

#### **6.1.4 Natural Environment Impacts**

The proposal does not have any negative natural environmental impacts, as there are no natural heritage features on the Site.

#### **6.1.5 Municipal Services Impacts**

There will be no negative impacts on the municipal system as the residential development is limited to low profile and will not add to the capacity in a significant way.



### **6.1.6 Social and/or Economic Conditions**

The proposed development does not negatively affect the social environment as the Site is in close proximity to major transportation networks, transit, parks, places of worship and community amenities.

The proposed development does not cause any public health and safety concerns.

The proposal represents a cost-effective development pattern that minimizes land consumption and servicing costs.

There will be no urban sprawl as the proposed development is within the existing settlement area and is an ideal development opportunity.

## **6.2 Conclusion**

The proposed development on the Site is appropriate and should be approved by the City of Windsor.

This PRR has shown that the proposed development is consistent with the PPS, conforms with the intent and purpose of the City of Windsor OP and ZBL, and represents good planning. The report components for this PRR have set out the following, as required under the City of Windsor OP:

*10.2.13.2 Where a Planning Rationale Report is required, such a study should:*

- (a) Include a description of the proposal and the approvals required;*
- (b) Describe the Site's previous development approval history;*
- (c) Describe major physical features or attributes of the Site, including current land uses(s) and surrounding land uses, built form and contextual considerations;*
- (d) Describe whether the proposal is consistent with the provincial policy statements issued under the Planning Act.*
- (e) Describe the way in which relevant Official Plan policies will be addressed, including both general policies and site specific land use designations and policies;*
- (f) Describe whether the proposal addresses the Community Strategic Plan;*
- (g) Describe the suitability of the Site and indicate reasons why the proposal is appropriate for this Site and will function well to meet the needs of the intended future users;*
- (h) Provide an analysis of the compatibility of the design and massing of the proposed developments and land use designations;*

- (i) Provide an analysis and opinion as to why the proposal represents good planning, including the details of any methods that are used to mitigate potential negative impacts;*
- (j) Describe the impact on the natural environment;*
- (k) Describe the impact on municipal services;*
- (l) Describe how the proposal will affect the social and/or economic conditions using demographic information and current trends; and,*
- (m) Describe areas of compliance and non-compliance with the Zoning By-law.*

**Planner's Certificate:**

I hereby certify that this report was prepared by Tracey Pillon-Abbs, a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.



**Tracey Pillon-Abbs, RPP  
Principal Planner**



**APPENDIX G – DRAFT BY-LAW**

B Y - L A W N U M B E R -2024

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600  
CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the            day of            , 2024.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of The Corporation of the City of Windsor enacts as follows:

1. By-law Number 8600 is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of the by-law and made part thereof, so that the zoning district symbol of the lands described in Column 3 shall be changed from that shown in Column 5 to that shown in Column 6:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	11	Pt Lot 105, Concession 2, designated as PART 1, Plan 12R-26347 [PIN 01360-0291 (LT)] and PART BLOCK 21 Plan 12M417 Windsor being PARTS 4, 5 & 6 Plan 12R27878 [PIN 01360-0301 (LT)] (located west of Tourangeau Road, between Plymouth Drive and Grand Marais Road East.)	-	RD2.1	H(2)RD2.2

2. THAT the holding (H) symbol SHALL APPLY to the land described as Pt Lot 105, Concession 2, designated as PART 1, Plan 12R-26347 [PIN 01360-0291 (LT)] and PART BLOCK 21 Plan 12M417 Windsor being PARTS 4, 5 & 6 Plan 12R27878 [PIN 01360-0301 (LT)] and that Zoning By-law Number 8600 be further amended by adding the following clause to Section 95.20:

(2) Registration of a Final Plan of Subdivision.

DREW DILKENS, MAYOR

CLERK

First Reading - , 2024  
Second Reading - , 2024  
Third Reading - , 2024

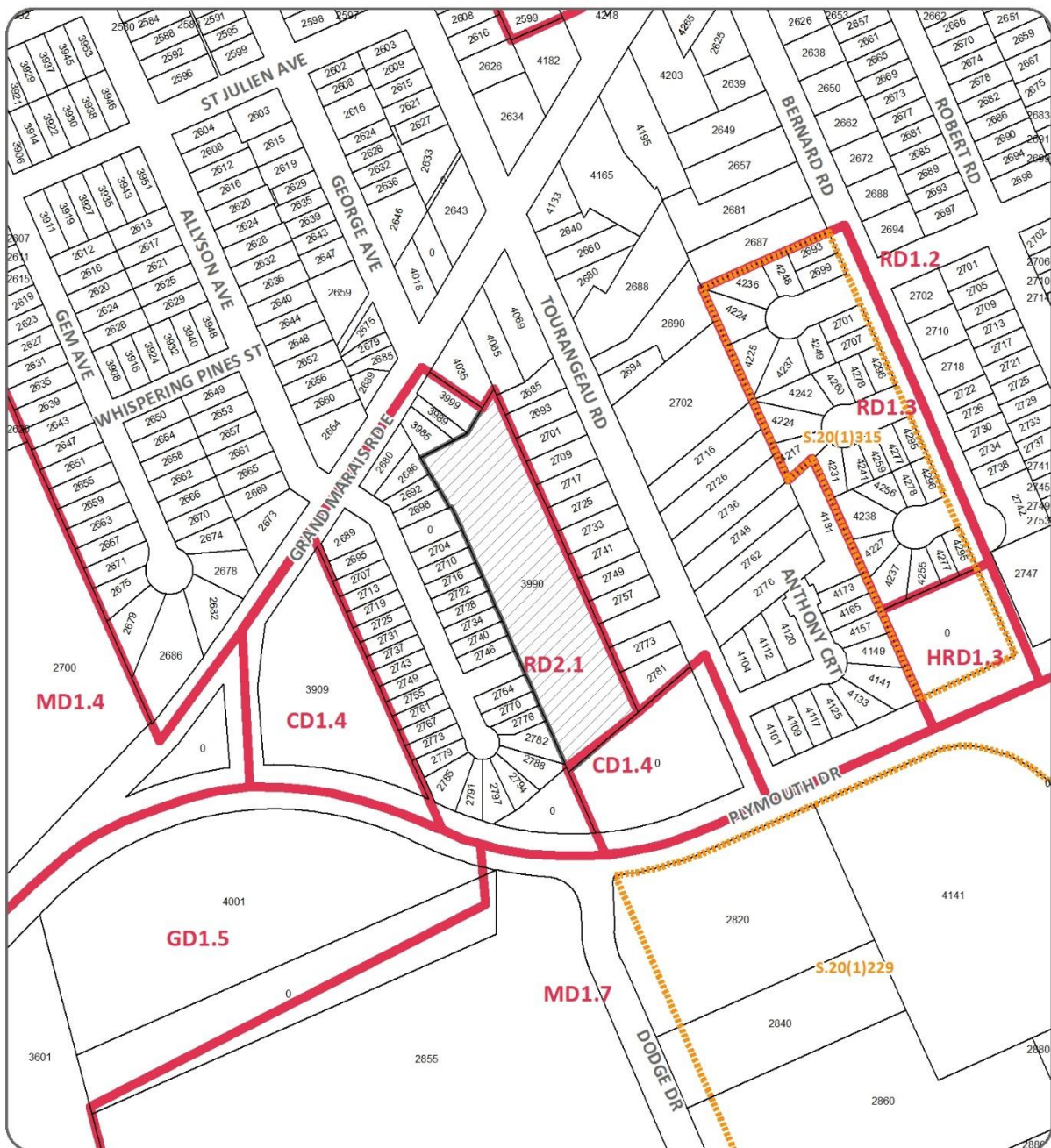
## SCHEDULE 2

1. By-law \_\_\_\_\_ has the following purpose and effect:

To amend the zoning of the land described as Pt Lot 105, Concession 2, designated as PART 1, Plan 12R-26347 [PIN 01360-0291 (LT)] and PART BLOCK 21 Plan 12M417 Windsor being PARTS 4, 5 & 6 Plan 12R27878 [PIN 01360-0301 (LT)], located west of Tourangeau Road, between Plymouth Drive and Grand Marais Road East, by changing the zoning from Residential District 2.1 to Residential District 2.2 (with a holding prefix) to facilitate the development of a residential plan of subdivision containing 4 townhome dwellings with a total of 28 townhome dwelling units on the subject land.

This amendment provides the opportunity for the creation of 4 blocks for low profile residential development in the form of townhouses on the subject lands. This amendment supports a more compact development and helps optimize the use of existing municipal infrastructure and public facilities in the subject area. The amendment provides the opportunity to develop the subject underutilized land without an amendment to the official plan.

2. Key map showing the location of the lands to which By-law \_\_\_\_\_ applies.



PART OF ZONING DISTRICT MAP 11

N.T.S.

## SCHEDULE 2

Applicant: Olivia Construction Homes Inc.



SUBJECT LANDS

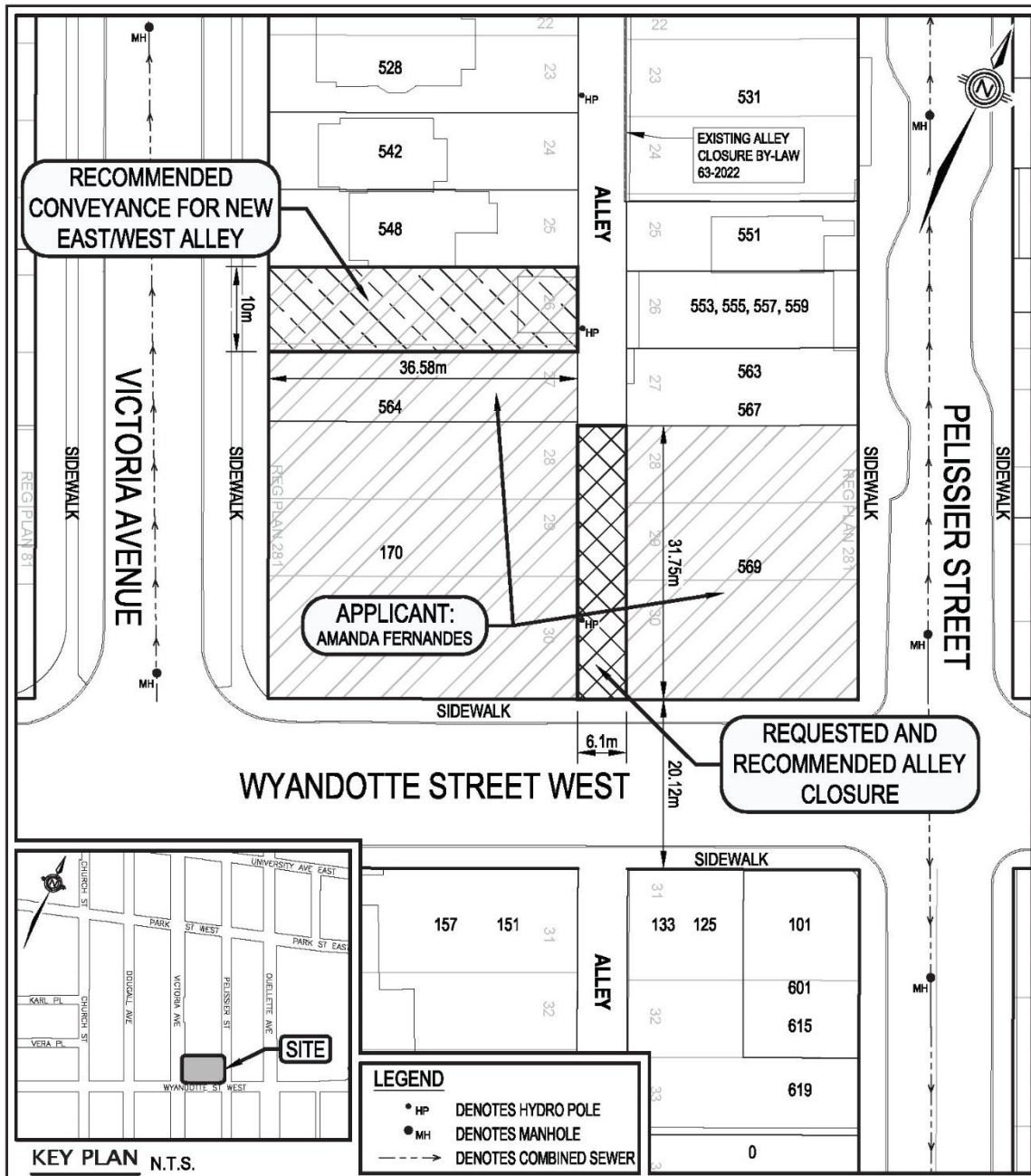
PLANNING & BUILDING DEPARTMENT



DATE : SEPTEMBER, 2024

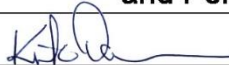
FILE NO. : SDN 003-24, SDN-7212 & Z-018-24, ZNG-7211

**APPENDIX "A"**  
**Drawing No. CC-1857**



THE CORPORATION OF THE CITY OF WINDSOR - ENGINEERING DEPARTMENT

**Proposed Closure of Part of the North/South Alley Between Victoria Avenue and Pelissier Street, North of Wyandotte Street West**

 Kirk Tamm, Manager of Geomatics	SCALE: 1:600	DATE: OCT 2024	REVISED: -	DWG. NO. <b>CC-1857</b>
	DWN BY: JM	CHKD BY: PJU / BN	REVISION NO.: -	

### APPENDIX "B" EIS Drawing - Aerial Photo




## STREET & ALLEY CLOSING (SAA/7034)

1:750 

APPLICANT : 2695185 ONTARIO LTD

 APPLICATION TO CLOSE

 RECOMMENDED TO BE AN OPEN RIGHT-OF-WAY

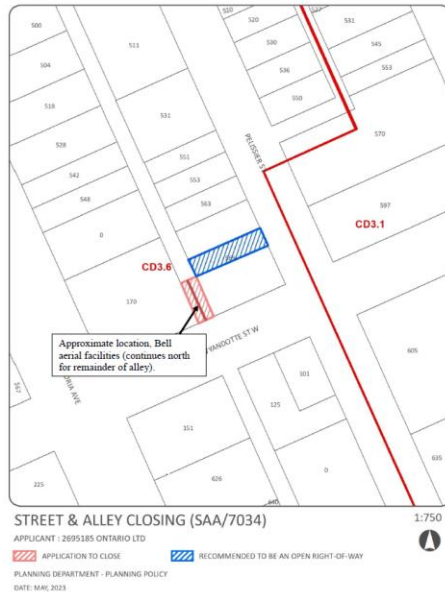
 ADJACENT PROPERTIES OWNED BY APPLICANT

PLANNING DEPARTMENT - PLANNING POLICY

DATE: SEPTEMBER, 2024

**APPENDIX "C"****Consultations with Municipal Departments and Utility Companies****BELL CANADA WSP****May 29, 2023**

Bell Canada requests a 3.0 m easement, to measure 1.5 m on either side of existing aerial facilities. If this cannot be accommodated, we request the easement over the entire closure area.

**July 24, 2023**

Bell has significant infrastructure within the alley and will require easement protection prior to the city's disposal of the lands.

**July 25, 2023**

Our facilities provide essential access to the network. Of major concern is the ability to access our equipment, particularly in the event of an interruption, or emergency. The easement would allow Bell Canada to restore service to regular telephone lines, alarm services, internet access, and most importantly ensure the continuity of 911 service.

**[Charleyne Hall - Bell Canada External Liaison, Right of Way & Indigenous Relations]****Note:**

EnWin Utilities is requesting that the closure be conditional on all their aboveground and below grade infrastructure being removed or relocated from the. Bell Canada's aboveground infrastructure is located on EnWin Utilities hydro poles, and consequently will need to be removed or relocated.

**COGECO**

No comments provided

**DEVELOPMENT ENGINEERING****May 2, 2024**

We are in favor of the development however our preference would be to close the N/S section of the alley as proposed but to not create the additional E/W section to Pelissier.

A new alley would have to be maintained by the City & since Environmental Services is in agreement with the property owner about their trucks travelling south down the N/S alley exiting onto Victoria though private property I don't see the reason to open a new alley to Pelissier St. I know you mentioned wanting to keep access for the residents to the north, but we close one end of alleys all the time, they can still enter and exit at Park Street.

Regardless of if we open a new alley to Pelissier St, our truck route will be through private property so we should have an easement for access in favor of the City. That would prevent the current and any future property owner from fencing or restricting that access as it would be registered on title. I would still have the owner sign a waiver as Anne-Marie noted but an easement will ensure us access through time. I see no need to grant easements for all the properties to the north, ours would ensure it is never blocked.

If we must have a public alley to Pelissier St as proposed, then maybe it can be created in the same manner as Maiden Lane - east of Ouellette where the underground parking area is private property and only the surface is ROW as we do not support an encroachment agreement for the parking garage under an alley and reconfiguring the underground parking would be a complete site redesign. But again, our preference would be to leave this area private property and only move forward with the closure area. If you want to ensure access is maintained to Pelissier and Victoria that can be done with the easement and would better for the City because it's the owner's responsibility to maintain their property. The owner would need to understand that vehicles using the alley may choose to exit via the easement areas as they won't be able to restrict access over those parts but if they were willing to put an alley through their property, I don't see an easement being any different for them.

**July 25, 2024**

I think this new plan resolves all the prior concerns. Engineering is in support.

**[Shannon Mills - Technologist III]****ENBRIDGE GAS**

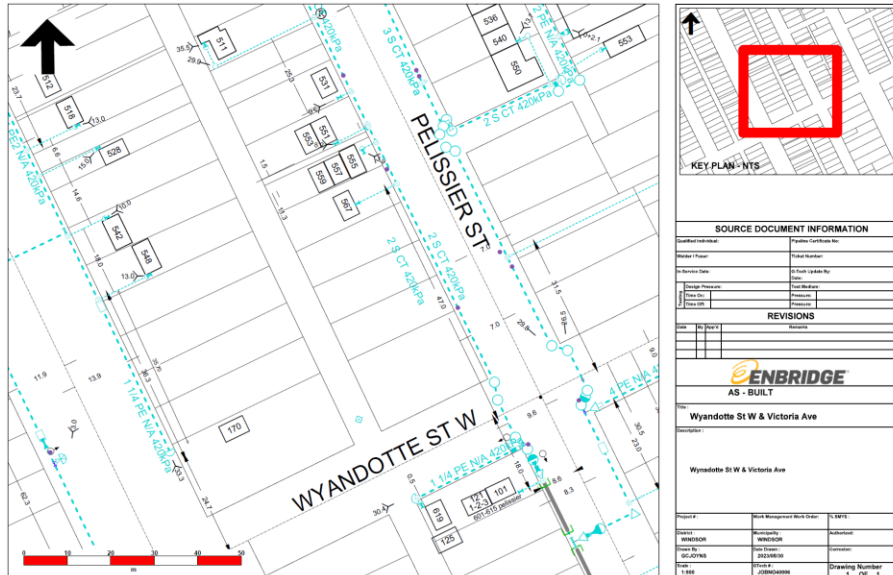
After reviewing the provided drawing at Wyandotte St E. & Victoria Ave. and consulting our mapping system, please note that Enbridge Gas has no active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Also, please note the following should you find any abandoned infrastructure in the area:



- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

### [Gord Joynson - Drafter Estimator]



## ENVIRONMENTAL SERVICES

### July 2024 Phone Conversation with Jim Leether (Manager of Environmental Services)

No objection contingent on the provision of a minimum clearance of 6.8 metres from grade to any aboveground wires in the area where refuse bins are emptied.

#### Note:

This matter will be addressed through the Site Plan Control process.

## ENWIN UTILITIES - HYDRO ENGINEERING

No objection contingent on the removal and relocation of EnWin's overhead and underground hydro plant.

Please be advised EnWin has underground high voltage conductors in the north / south alley between Victoria and Pelissier.

Please be advised EnWin has overhead high voltage power lines in the north / south alley between Victoria and Pelissier.

The applicant will be required to cover all costs associated with the removal and/or relocation of EnWin's underground and overhead hydro plant customer to contact & coordinate with EnWin.

Prior to working in these areas, we suggest notifying your contractor and referring to the *Occupational Health And Safety Act* and regulations for construction projects to confirm clearance requirements during construction. also, we suggest referring to the *Ontario Building Code* for required clearances for new building construction.

**[Nillavon Balachandran - Hydro Engineering Technologist]**

**ENWIN UTILITIES - WATER ENGINEERING**

Water Engineering has no objections.

**[Bruce Ogg - Water Project Review Officer]**

**LEGAL DEPARTMENT**

**June 23, 2023**

It should be a condition of site plan that we do a land exchange rather than gratuitous conveyance. Therefore, no price will be set for the alley.

**[Stephanie Santos - Coordinator of Real Estate Services]**

**July 24, 2024**

I have now had the opportunity to review the revised conceptual plan.

From my perspective, there are no legal issues with the revised plan. We needed them to propose a solution allowing for access to Victoria Avenue from the existing north/south alley. This has now been provided.

It is my suggestion that should we seek Council approval for this closure, that it be conditional upon the owner first constructing the new east/west alley to the City's specifications. Once approved by the City, then we will pass the by-law to close and convey. I will assist with the wording of these recommendations in the Council report, should we get to that point.

**[Kate Tracey - Senior Legal Counsel]**

**MNSi**

No comments provided

**PARKS & FACILITIES**

No issues from Natural Areas

**[Karen Alexander - Naturalist & Outreach Coordinator]**

No issue from Parks Design & Development as well

**[Sherif Barsom - Landscape Architect]**

No issue for Forestry as well

**[Yemi Adeyeye - City Forester / Manager Forestry & Natural Areas]**

### **PLANNING DEPARTMENT - LANDSCAPE ARCHITECT**

No objections from a Landscape Architectural or Urban Design perspective.

**[Stefan Fediuk - Landscape Architect]**

### **PUBLIC WORKS - ENGINEERING**

ROW agrees with Transportation Planning's comments. We do not support any parking in the alley. The original drawings showed it as a loading space, not parking spaces. The parking will impede larger vehicles trying to access the alley.

**[Adam Pillon - Manager of Right-of-Way]**

### **PUBLIC WORKS - OPERATIONS**

**April 21, 2023**

From a maintenance perspective, we would not oppose the "relocation" of the alley to another portion of one of the subject properties. Any costs to redirect affected drainage, paving of the new alley to suit, utility relocation (if any) would be the responsibility of the development. New pavement composition of the alley would be as per City Standards and specifications.

Any turning radii etc. necessary to accommodate garbage and recycling would also be required subject to Anne Marie's comments on whether this would be even possible, from her collection's perspective.

**[Roberta Harrison - Coordinator Maintenance]**

**November 8, 2023**

We do not have city sewer in this alley. When purchased, the drainage for this portion of the alley would be the new owner's responsibility therefore I have no issue with it.

**[Phong Nguy - Manager of Contracts, Field Services]**

### **ROGERS COMMUNICATIONS**

No comments provided

**TELUS COMMUNICATIONS**

Existing TELUS structure will need to be relocated.

Previously had conversation with Phillip Fernandes late last year and gave him a high level relocation cost.

**[Fred Sua - TELUS Communications]**

**TRANSPORTATION PLANNING**

Transportation Planning has no comment on the attached Conceptual Plans for the partial closure of the subject alley.

**[Ellie Mehrilou - Transportation Planner I]**

**TRANSIT WINDSOR**

Transit Windsor has no objections.

**[Jason Scott - Supervisor, Planning]**

**WINDSOR FIRE**

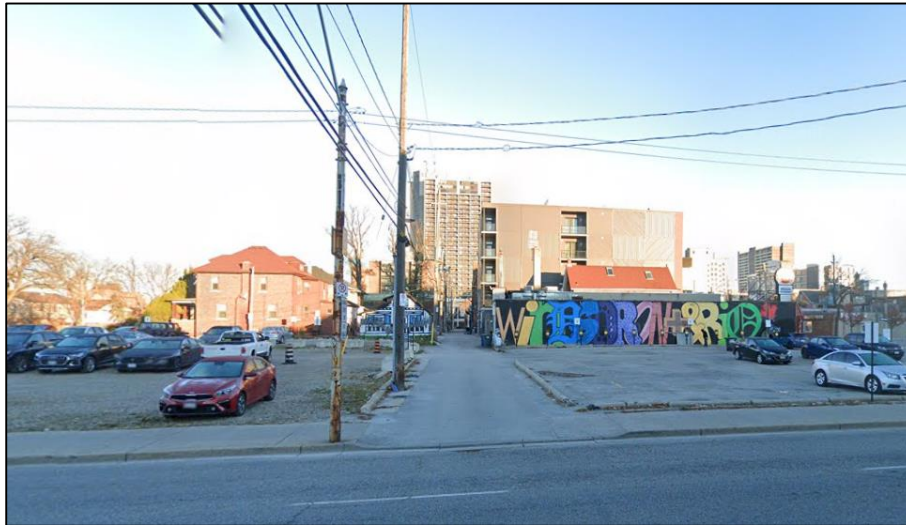
No comments provided

**WINDSOR POLICE**

The Windsor Police Service has no concerns or objections with the closure of this section of alley and the creation of the access route. It is our understanding this closure is a necessary component of a redevelopment plan for the broader property. The outcome from these changes will not impair the ability of the police to respond to incidents or carry out service delivery, so there is no anticipated impact on public safety.

**[Barry Horrobin - Director of Planning & Physical Resources]**

**APPENDIX "D"**  
**Site Photos (Google Street View - November 2023)**



*Figure 1 - Looking north towards alley from Wyandotte St W (170 Wyandotte St W on left)*



*Figure 2 - Looking east towards location of proposed E/W Alley from Victoria Ave (548 Victoria Ave on left)*

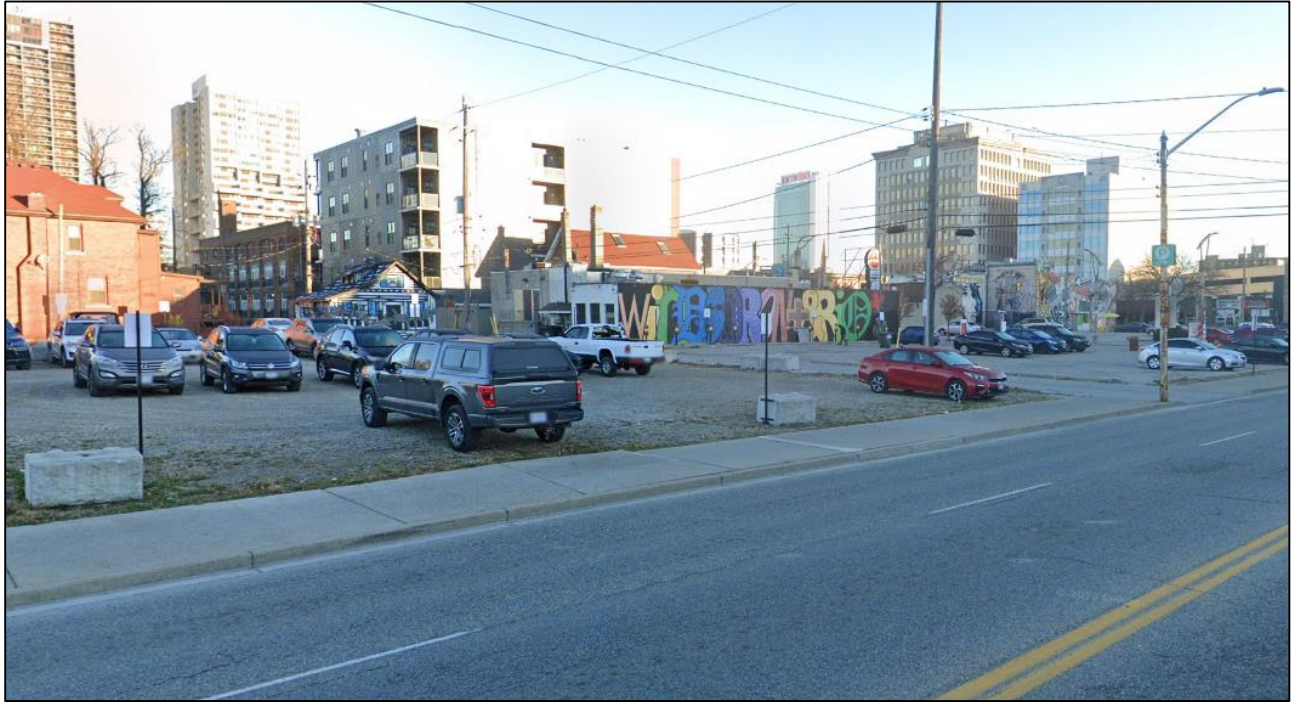


Figure 3 - Looking northeast towards alley from Wyandotte St W (170 Wyandotte St W on left)

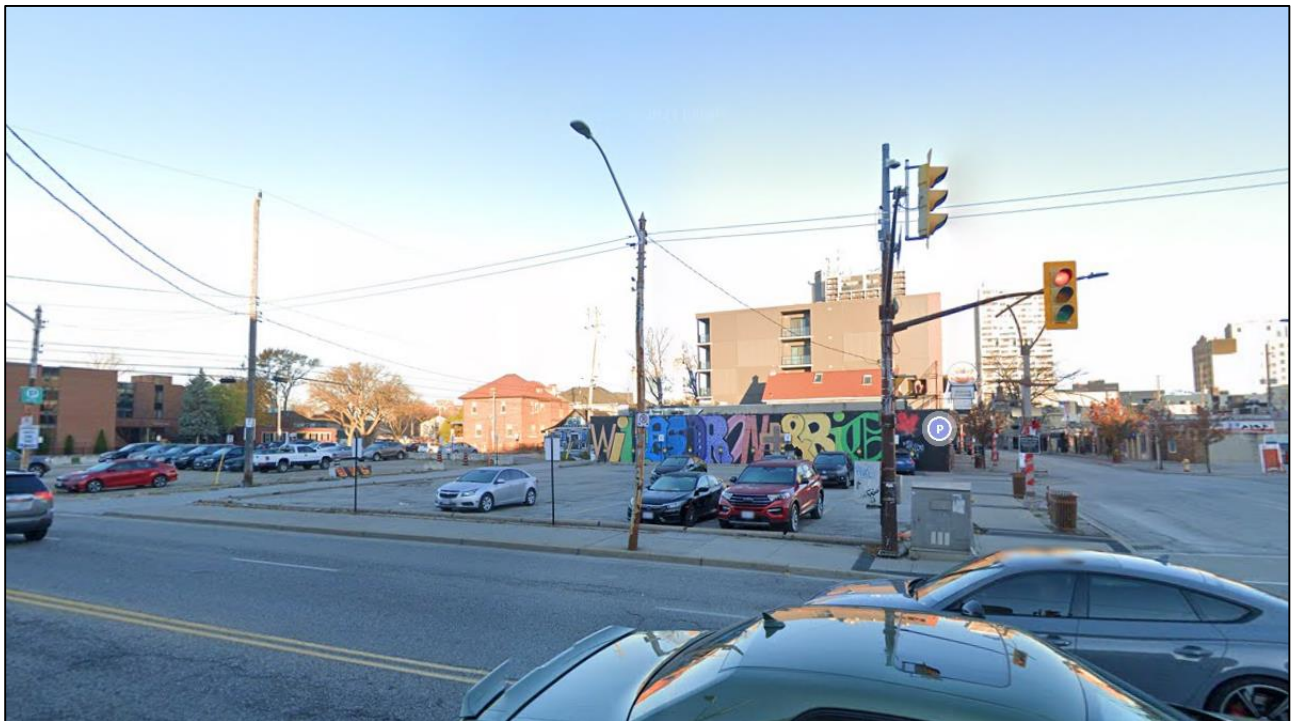


Figure 4 - Looking northwest towards alley from Wyandotte St W (Pelissier St on right)



Figure 5 - Looking northeast towards location of proposed E/W alley from Victoria Ave (170 Wyandotte St W on right)

## **APPENDIX “E”**

### **Classification of Alleys and Suitability for Closure**

#### **Classification of Public Rights-of-Ways:**

Currently streets and alleys fall into four classifications on the basis of their usefulness:

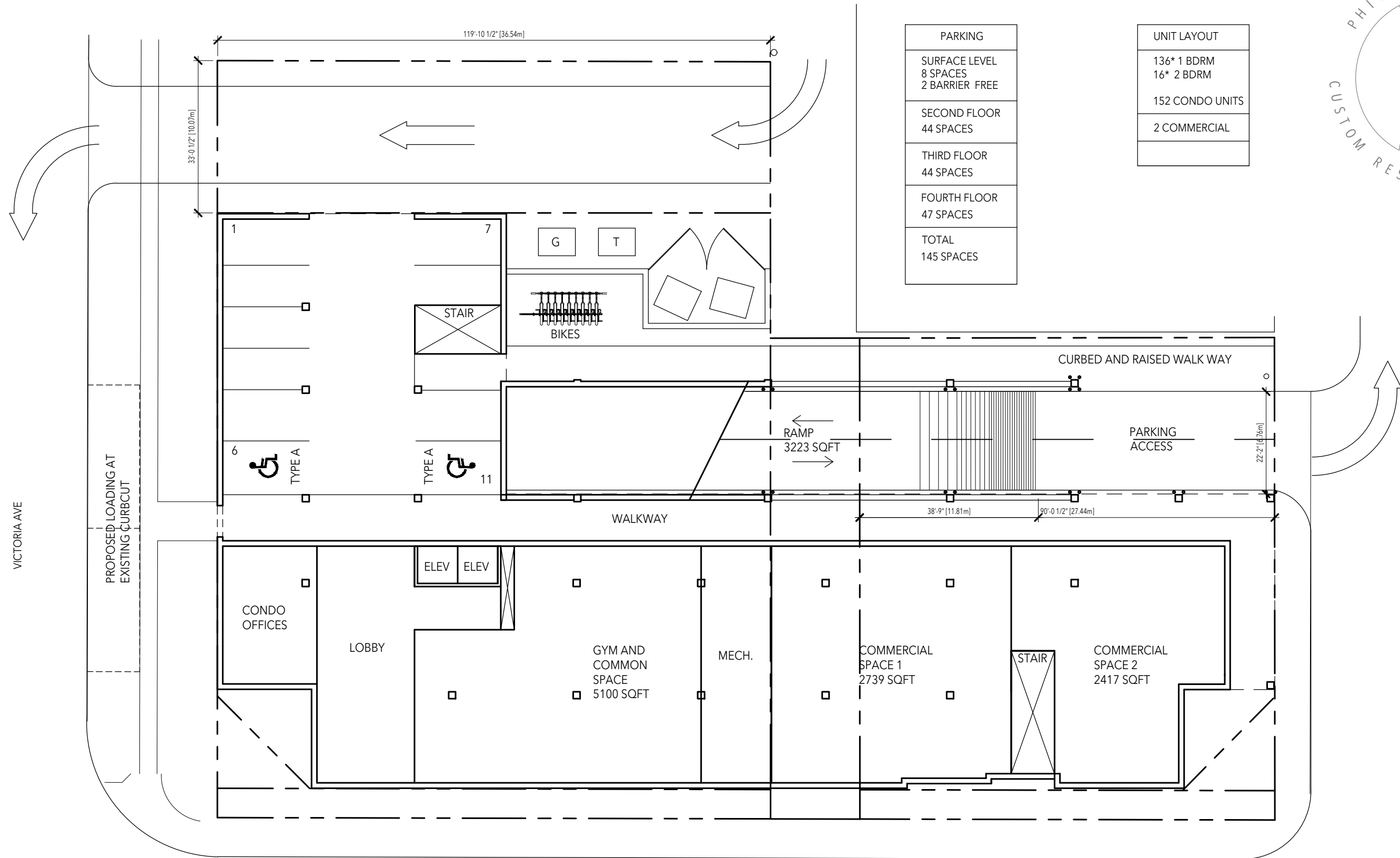
- 1) Alleys that are **indispensable**. These would be alleys serving commercial properties and properties fronting on heavily traveled streets i.e. major arterial routes and alleys which contain sewers and must remain accessible for servicing; alleys or streets which serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive; and, alleys which contain Fire Department connections that are deemed to be necessary for firefighting access.
- 2) Alleys that, **have some usefulness**, are nevertheless dispensable and may or may not be a complete liability.
- 3) Alleys that appear to serve **no useful purpose**, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
- 4) Alleys lying in Holding zones and other similar undeveloped areas where the alley system is **clearly obsolete** and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

#### **Suitability for Closing:**

Following are the criteria and suitability for closing alleys in each of the above classifications:

- 1) Indispensable alleys should **not be closed**, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof. They are essential from the viewpoint of fire protection, police protection, emergency services (i.e. ambulance) and loading or unloading of goods, refuse collection, servicing of blocked sewers and utility services. Without such alleys, the above noted services would at least be more costly if not impossible to complete or adequately access; and would noticeably interfere with street traffic, thereby reducing the access capacity of the adjacent arterial, collector, or street for business.
- 2) Alleys having some usefulness should **be considered for closing** only upon request of abutting owners rather than by encouragement of the City.
- 3) Alleys that serve no useful purpose should **be closed** if at all possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
- 4) Alleys that are clearly obsolete should **not be closed** unless there is a municipal need or specific development proposals acceptable to the City are submitted.





PARKING
SURFACE LEVEL 8 SPACES 2 BARRIER FREE
SECOND FLOOR 44 SPACES
THIRD FLOOR 44 SPACES
FOURTH FLOOR 47 SPACES
TOTAL 145 SPACES

UNIT LAYOUT
136* 1 BDRM 16* 2 BDRM
152 CONDO UNITS
2 COMMERCIAL

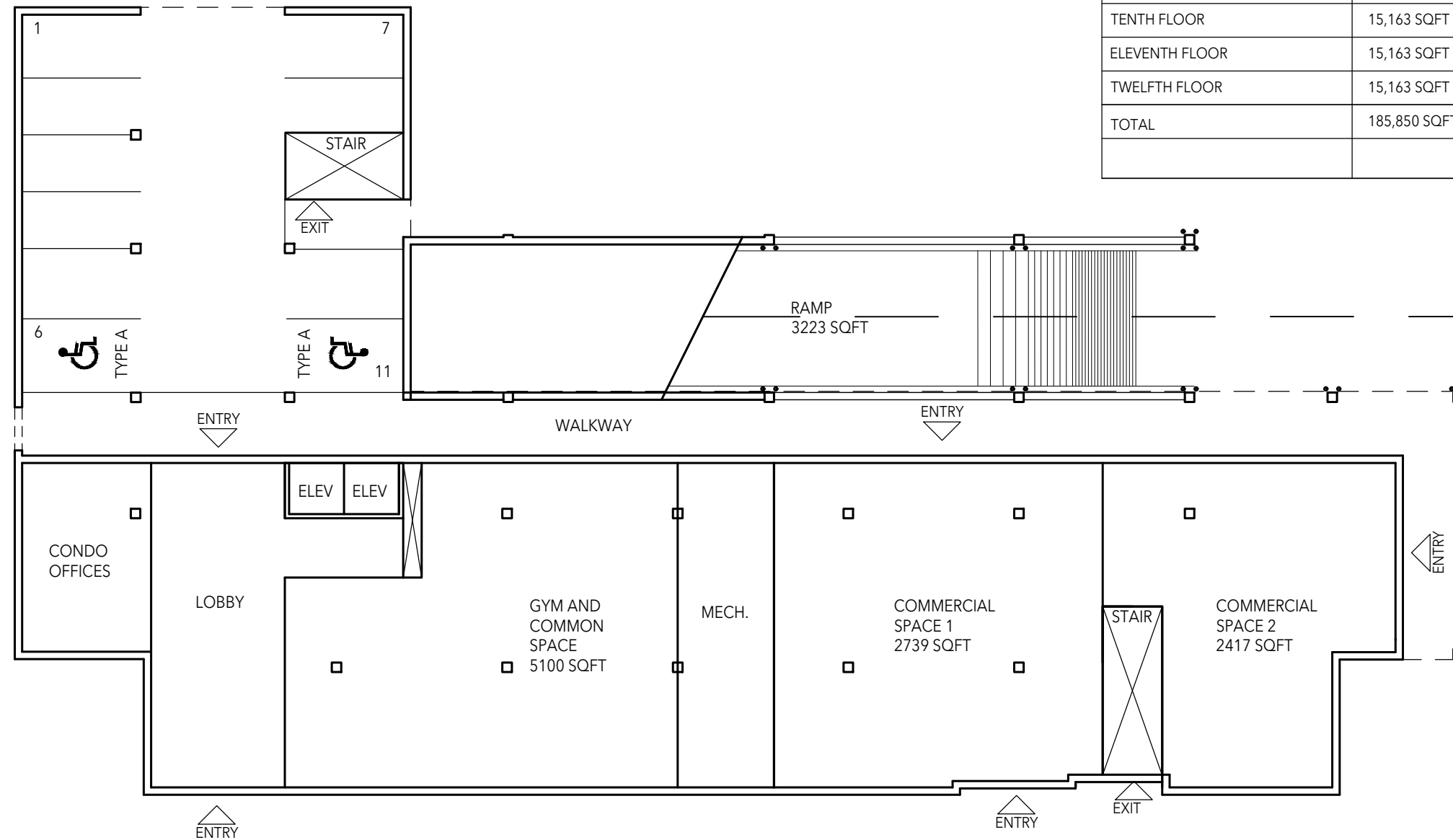
WYANDOTTE

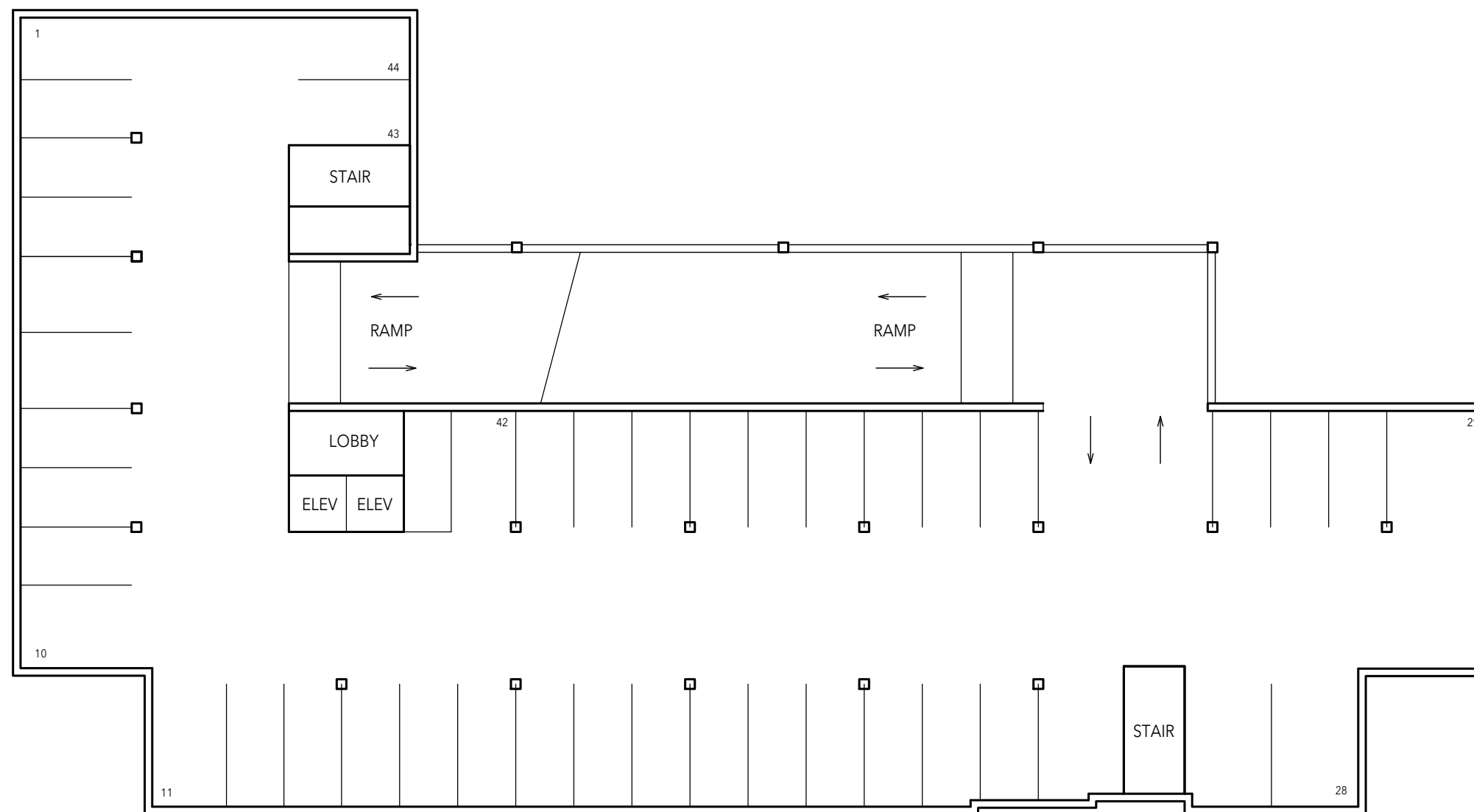
# S I T E P L A N

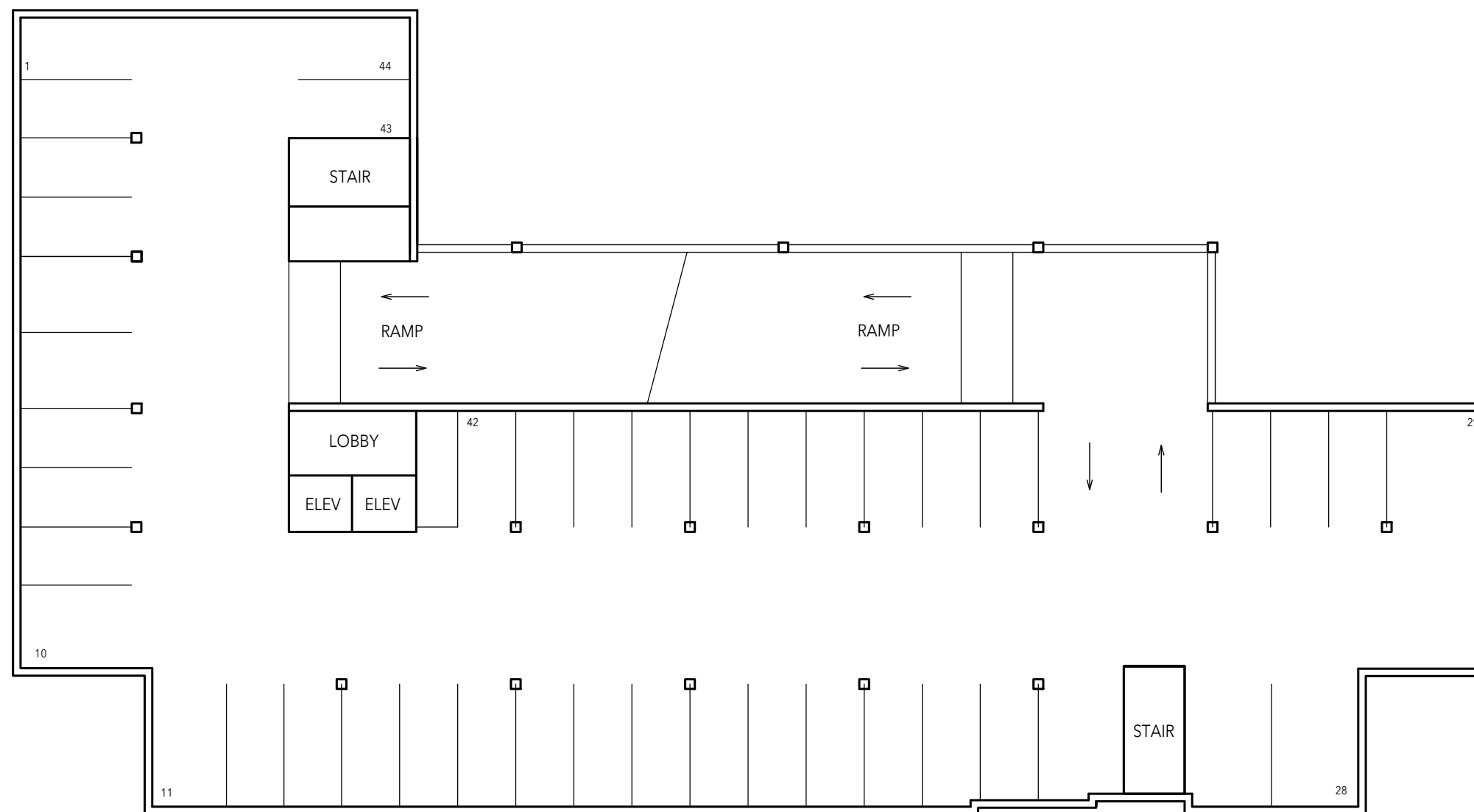
SCALE: NTS

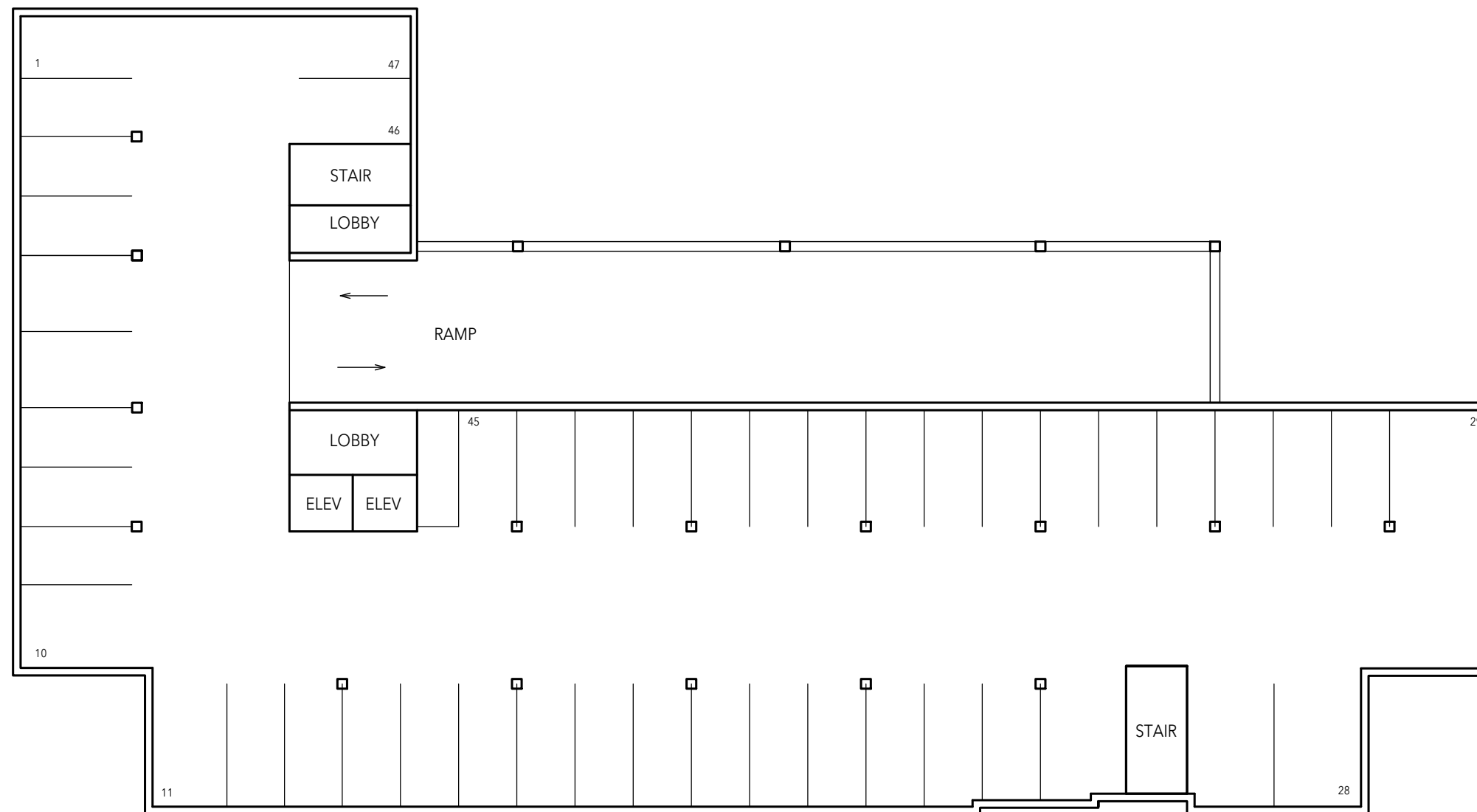
UNIT MATRIX	
COMMERCIAL SPACES	2
CONDOMINIUM LOBBY AND ENTRY 2097 SQFT	1
COMMON AREA 3540 SQFT	1
TOTAL	4

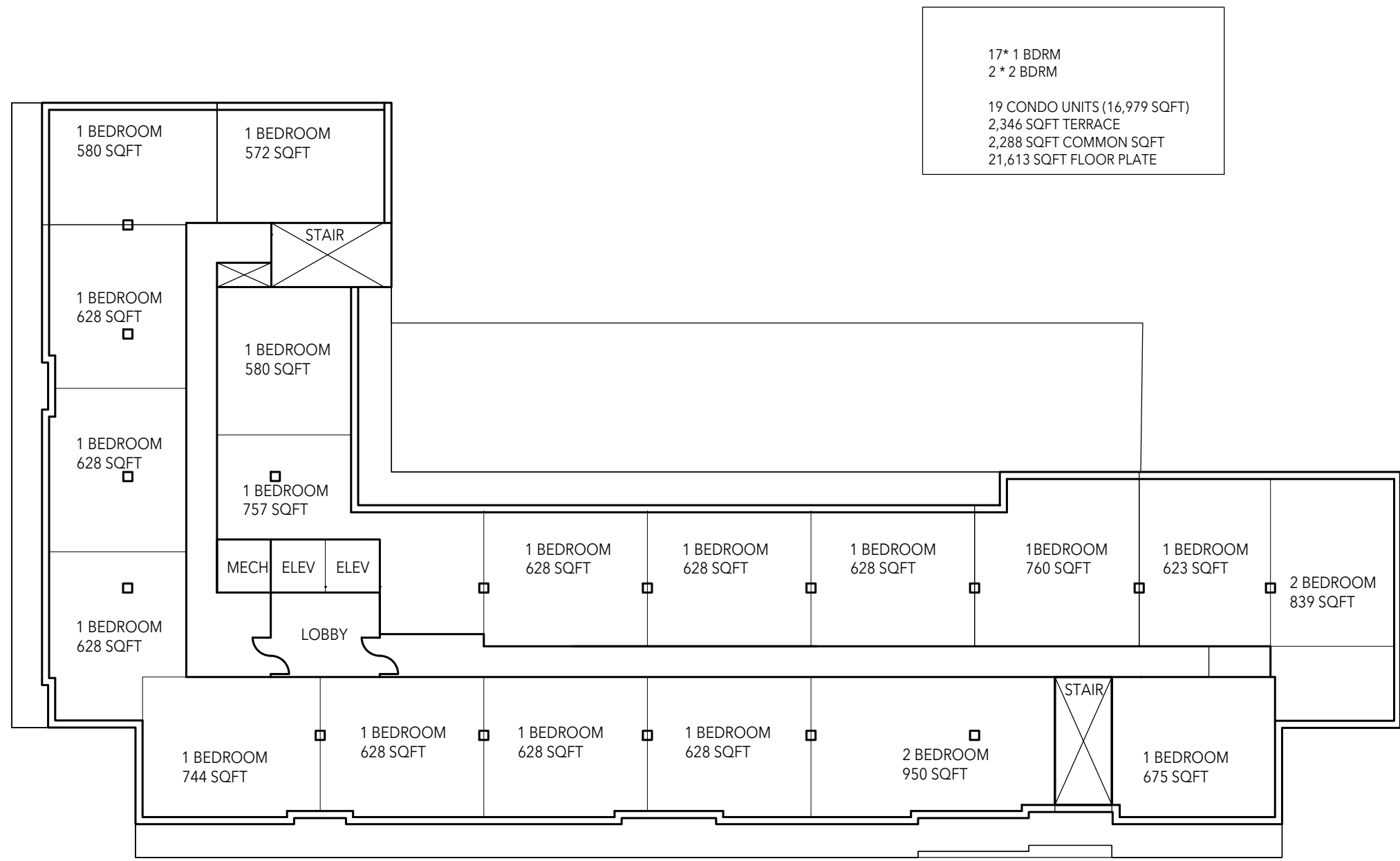
BUILDING FOOTAGE	
FIRST FLOOR (RAMP 3223 SQFT)	14,315 SQFT
SECOND FLOOR (PARKING)	20,553 SQFT
THIRD FLOOR (PARKING)	20,553 SQFT
FOURTH FLOOR (PARKING)	20,553 SQFT
FIFTH FLOOR	18,898 SQFT
SIXTH FLOOR	15,163 SQFT
SEVENTH FLOOR	15,163 SQFT
EIGHTH FLOOR	15,163 SQFT
NINTH FLOOR	15,163 SQFT
TENTH FLOOR	15,163 SQFT
ELEVENTH FLOOR	15,163 SQFT
TWELFTH FLOOR	15,163 SQFT
TOTAL	185,850 SQFT









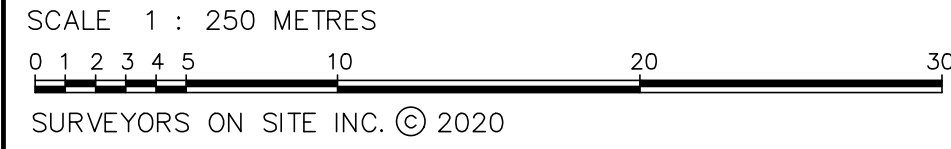


# FIFTH FLOOR PLAN

SCALE: NTS



PLAN OF SURVEY SHOWING TOPOGRAPHICAL DETAILS OF ALL OF LOTS 26 TO 30 (INCLUSIVE) EAST SIDE OF VICTORIA AVENUE ALL OF LOTS 28, 29 & 30 WEST SIDE OF PELISSIER STREET REGISTERED PLAN 281 CITY OF WINDSOR COUNTY OF ESSEX



LEGEND

- MONUMENT PLANTED
- MONUMENT FOUND
- IRON BAR
- ⊕ CUT CROSS
- PK PK NAIL
- M MEASURED
- S SET
- P1 REGISTERED PLAN 281
- P2 POS BY STUBBERFIELD & HARTLEY LTD. AT THE OFFICES OF VERHAEGEN, STUBBERFIELD, HARTLEY, BREWER, BEZAIRE INC. INTERNAL FILE NUMBER E-281-W/S PEL.-2. PLAN 12R-8666
- P3 BUILDING LOCATION SURVEY CLARKE, MATTHEWS LIMITED AT THE OFFICES OF CLARKE SURVEYORS, INTERNAL FILE NUMBER L-281-2-E/S VICTORIA AVE. PLAN 12R-8666
- P4 CLARKE SURVEYORS INC.
- 1201 ORIGIN UNKNOWN
- OU ANCHOR
- AN BOLLARD
- BL BIKE RACK
- BR CATCH BASIN - ELEVATION AT TOP CENTRE
- CB CLEANOUT
- CO FIRE HYDRANT
- FH GAS METER
- GM GAS VALVE
- GV JUNCTION BOX
- JB LIGHT STANDARD
- LS MANHOLE - ELEVATION AT TOP CENTRE
- MH REAR YARD CATCH BASIN - ELEVATION AT TOP CENTRE
- RYCB TRAFFIC LIGHT
- TL UTILITY POLE
- UP WATER VALVE
- WV UNDERGROUND BELL LINE
- BE UNDERGROUND FIBER OPTIC CABLE
- FOC UNDERGROUND GAS LINE
- G UNDERGROUND HYDRO LINE
- OH OVERHEAD UTILITY WIRES
- W UNDERGROUND WATER LINE

NOTES

DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

DISTANCES ON THIS PLAN ARE HORIZONTAL GROUND DISTANCES AND CAN BE CONVERTED TO GRID DISTANCES BY MULTIPLYING BY THE AVERAGE COMBINED SCALE FACTOR OF 0.99991090.

BEARINGS ARE UTM GRID DERIVED FROM NETWORK RTK GPS OBSERVATIONS (SMARTNET) ON MONUMENTS (A) AND (B) AS SHOWN HEREON, HAVING A GRID BEARING OF N24°18'20"W, NAD83 CSRS (2010.0) AND ARE REFERRED TO THE CENTRAL MERIDIAN OF UTM ZONE 17 (81° W LONGITUDE).

SITE BENCHMARK SHOWN HEREON AS THE TOP NUT OF A FIRE HYDRANT LOCATED ON THE NORTH EASTERLY CORNER OF WYANDOTTE STREET WEST AND VICTORIA AVENUE AS HAVING AN ELEVATION OF 184.011m.

ELEVATIONS ARE GEODETIC AND REFERRED TO THE CANADIAN GEODETIC VERTICAL DATUM (CGVD28) BY DIRECT MEASUREMENT TO A REAL TIME NETWORK (SMARTNET).

FOR BEARING COMPARISONS, THE FOLLOWING ROTATIONS WERE APPLIED:  
 P2 - 0°26'50" CLOCKWISE.  
 P3 - 1°02'50" CLOCKWISE.  
 P4 - 1°16'40" CLOCKWISE.

COORDINATES BELOW ARE DERIVED FROM NETWORK RTK GPS OBSERVATIONS (SMARTNET) AND ARE REFERRED TO UTM ZONE 17, NAD83 (CSRS) (2010.0).

COORDINATES COMPLY WITH THE URBAN ABSOLUTE ACCURACY PER SEC. 14(2) OF O.REG. 216/10.

POINT ID	NORTHING	EASTING
(A)	4686613.23	332064.80
(B)	4686684.79	332032.48
(C)	4686603.00	331992.72
(D)	4686691.04	331953.05

COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS OR BOUNDARIES SHOWN ON THIS PLAN.

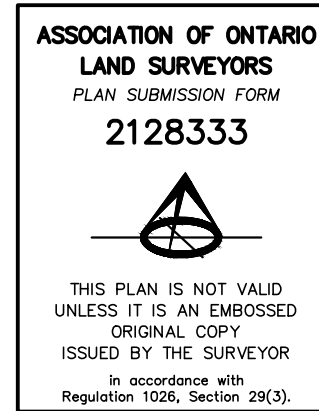
SURVEYOR'S CERTIFICATE

I CERTIFY THAT:

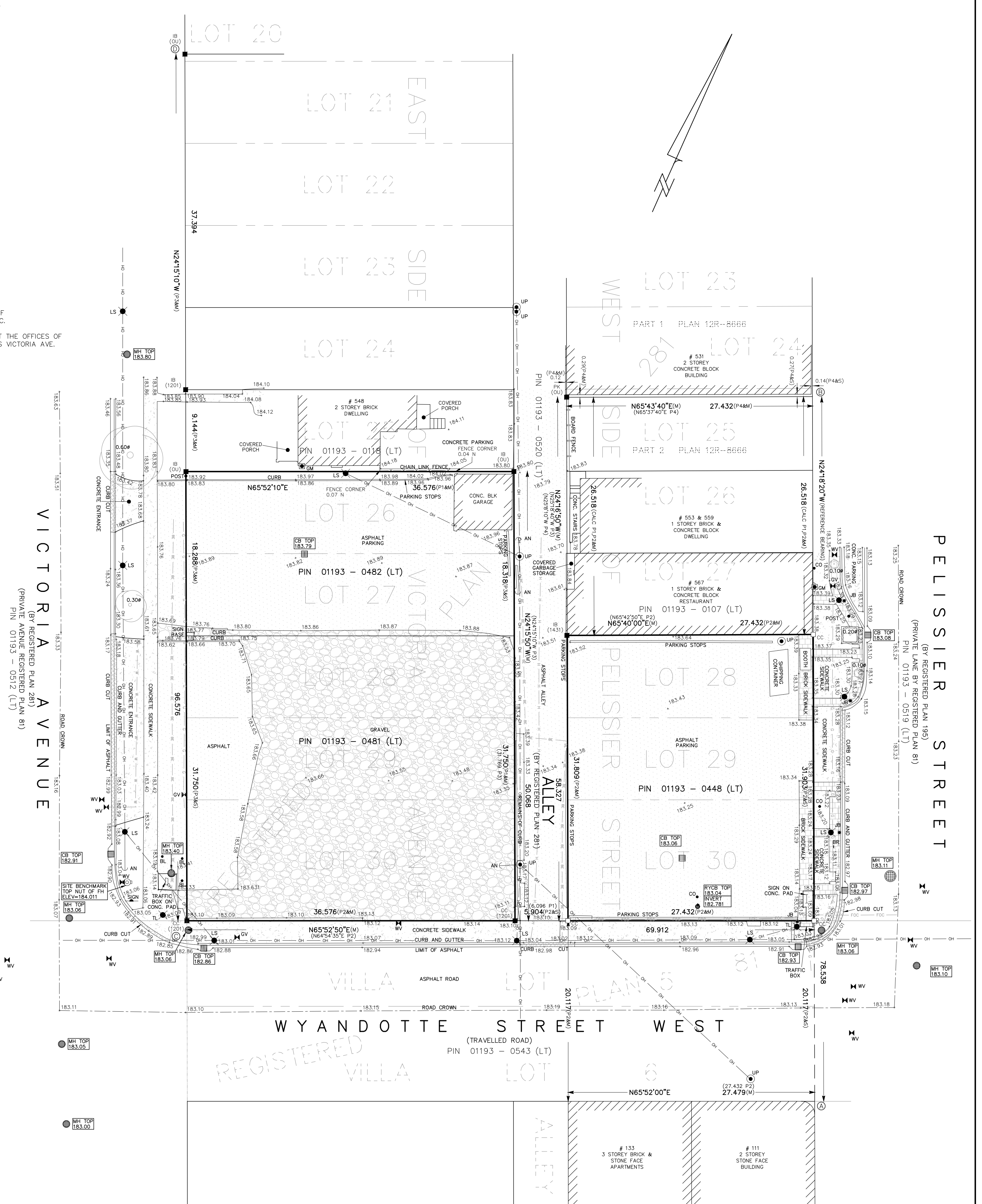
- THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT, THE LAND TITLES ACT AND THE REGULATIONS MADE UNDER THEM.
- THE SURVEY WAS COMPLETED ON THE 25th DAY OF OCTOBER, 2020.

NOVEMBER 6, 2020

*Ryan W. Secuin*  
 RYAN W. SECUIN  
 ONTARIO LAND SURVEYOR



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 WINDSOR, ONTARIO  
 N8W 3R8  
 519-818-0767  
 www.surveyorsonsite.com



Project Phase 1 (2020/2021)			
Property	Change Order / Cost Allowance #	Change Order Cost	Description
Parking Garage 1	CA 001	\$38,117	Changed design to City requested fixture for enhanced safety/security (Traffic)
Parking Garage 2	CA 002	\$11,883	Changed design to City requested fixture for enhanced safety/security (Traffic)
Parking Garage 2	CO 001	\$1,191	Changed design to City requested fixture for enhanced safety/security (Traffic)
Fire Hall 5 and WFCU Centre	CO 002	\$4,407	Additional work (scope additions)
Windsor International Aquatic and Training Centre	CO 003	\$24,252	Additional controls
Lou Romano Water Reclamation Plant	CO 004	\$2,969	Added lights for enhanced safety (Pollution Control)
Roseland Golf and Curling Club	CO 005	\$37,479	Redesign for 48 Lights in the Curling Rink
Forest Glade Community Centre	CO 006	\$50,656	Redesign for 72 Lights for the 2 Pads (6L T5HO to 213W LED HB - 3,000 hours)
Lou Romano Water Reclamation Plant	CO 007	-\$2,969	Removed the CO for larger scope in CO 008
Lou Romano Water Reclamation Plant	CO 008	\$27,685	Additional lights/plugs added for safety in the plant (Pollution Control)
Lou Romano Water Reclamation Plant	CO 009	\$1,097	Additional plugs added for safety (Pollution Control)
Lou Romano Water Reclamation Plant	CO 010	\$1,043	Additional lights/plugs added for safety in the plant (Pollution Control)
Mackenzie Hall	CO 011	\$5,655	1,000W Halogen lights to new 8" Downlights (50% not operational)
Windsor International Aquatic and Training Centre	CO 012	\$1,112	New control panel
Windsor International Aquatic and Training Centre	CO 013	\$29,255	Additional lights added around diving board as requested by staff (Parks and Recreation)
Windsor Joint Justice Facility	CO 014	\$61,263	Cost increase - 3 year delay (Infrastructure Ontario)
Windsor International Aquatic and Training Centre	CO 015	\$8,461	Recessed round fixtures in lobby retrofitted from T5 to T5 LED
		\$303,555	

Project Phase 2 (2022/2023)			
Property	Change Order / Cost Allowance #	Change Order Cost	Description
Capri Pizzeria Recreation Complex	CA 001	\$13,155	Adder of 48 fixture to South Windsor Arena (scope addition)
Transit Windsor	CA 002	\$31,482	Voltage Change
Lanspeary Park	CA 003	\$1,635	Voltage Change
Lanspeary Park	CA 004	\$1,751	Replace Damaged Wiring
Capri Pizzeria Recreation Complex	CA 005	\$1,975	Lighting Controls Upgrade
Capri Pizzeria Recreation Complex	CO 001	\$14,596	Lighting Controls Upgrade
Capri Pizzeria Recreation Complex	CO 002	\$37,970	Upgrade rest of Building to LED (scope addition)
Lou Romano Water Reclamation Plant	CO 003	\$17,024	61 new 4' LED vapor lights and 8 x new 8' LED vapor lights due to rusted fixtures (Pollution Control)
Mackenzie Hall	CO 004	\$2,954	Scaffold and wood built around antique chest
Art Windsor-Essex	CO 005	\$1,246	Low Voltage wire and dimmer for pot lights
Transit Windsor	CO 006	\$6,839	Take down lenses and tie wrap 497 lenses (Transit)
Transit Windsor	CO 007	\$1,132	Demo light and repair skylight including scaffold
Lou Romano Water Reclamation Plant	CO 008	\$5,265	Replace 20 fixtures with new LED Vapor. Premium for new vs retrofit (Pollution Control)
Capri Pizzeria Recreation Complex	CO 009	\$6,662	Replace 45 fixtures in lobby with new flat panel fixtures.
Lou Romano Water Reclamation Plant	CO 010	\$1,254	Temporary Lunchroom and Bathroom (worker safety requirement)
Little River Pollution Control Plant	CO 011	\$2,629	Temporary Lunchroom and Bathroom (worker safety requirement)
400 City Hall Square East	CO 012	\$975	Supply 16 New lenses
Art Windsor-Essex	CO 013	\$2,254	Supply Install 6 LED corn lights - remove old pole head fixtures (scope addition)
Little River Pollution Control Plant	CO 014	\$14,037	Supply install exterior wall packs and pole lights and reassemble
Roseland Golf and Curling Club	CO 015	\$15,009	Supply install exterior new track lighting and down lights (Roseland Golf & Curling Club)
Capitol Theatre	CO 016	\$1,940	Supply install screw in lamps, LED surface mount round fixtures
Lou Romano Water Reclamation Plant	CO 017	\$9,561	Installation of fixture and scaffold on mezzanine level (worker safety requirement)
Capri Pizzeria Recreation Complex	CO 018	\$6,452	43 Pot Light Retrofit Kits (scope addition)
Lou Romano Water Reclamation Plant	CO 019	\$7,182	Additional outdoor and signage lighting and hard to reach lights that were not operational (scope addition)
Adie Knox Herman Recreation Complex	CO 020	\$59,429	Installation of new High Bay Lighting in Arena along with Wavelinx Lighting Control
400 City Hall Square East	CO 021	\$46,262	Government of Canada Adder (scope addition)
400 City Hall Square East	CO 022	\$24,562	Lighting Controls Upgrade (Human & Health Services)
400 City Hall Square East	CO 023	\$2,625	LED Touch Screen (Human & Health Services)
400 City Hall Square East	CO 024	\$12,005	Lighting Control Wiring (Human & Health Services)
		\$349,860	