

1. SUBJECT: DRIVEWAY APPROACHES**2. DEFINITION**

Driveway Approach - The term driveway approach refers to the paved portion of a driveway within the public right-of-way which provides for access to private property from the municipal roadway.

Delineating Structure – Refers to any traversable or non-traversable raised and/or textured structure used to define lanes of traffic at access driveways into and out of a specific destination including left turn lanes, while in some cases discouraging travel in lanes of oncoming traffic. Examples include but are not limited to, median islands, rumble strips, etc.

3. DRAWING

AS-203 – Commercial Approach – No Curb

AS-204 – Commercial Drive - Concrete

AS-221 – Residential Drive - Asphalt

AS-222 – Residential Drive - Concrete

4. BEST PRACTICE

Street opening permits are required from the Engineering Department for all new driveway approaches or upgrades to existing driveways.

Driveway approaches for commercial properties shall be constructed as follows:

- Where the municipal road has curbs and gutters, OR where curbs and gutters are to be constructed prior to construction of the driveway approach, the driveway approach shall be constructed of concrete in accordance with AS-204.
- Where the municipal road does not have curbs and gutters AND curbs and gutters are not to be constructed prior to the construction of the driveway approach, at the discretion of the City Engineer, the driveway may be constructed of asphalt in accordance with AS-203. If a multi use trail cuts through the approach, the trail shall be constructed of concrete in accordance with AS-403.
- All driveway approaches shall be constructed using straight flare with raised curbs terminated at the property line. **Where deviation from this standard is proposed, the alternative design must conform to TAC guidelines**, to the satisfaction of the City Engineer.
- No “delineating structures” are permitted on a driveway approach within the right-of-way. If installed, they must be constructed entirely on private property. The only exception being BP2.1.2 Porkchop Islands.

For residential driveway approaches, the following rules apply:

- All driveway approaches shall be hard surfaced and be constructed in accordance with AS-221 or AS-222 as applicable.
- Where there is an existing sidewalk on the City right-of-way and the sidewalk is close to or at the property line, the minimum requirement for paving is 1.2m (4 feet) from the back of the sidewalk, regardless of whether this extends onto private property. This reduces the chances that granular material from the private portion of the driveway will migrate onto the City sidewalk presenting an unsafe condition.

- Driveway approaches may be constructed with or without a flare with maximum driveway approach width occurring at the curb cut (width in accordance with the street opening permit).

5. RELATED BEST PRACTICES

BP2.2.1 – Driveway Requirement Policies – Residential
Sidewalk/Driveway Construction

6. RELATED CITY STANDARDS

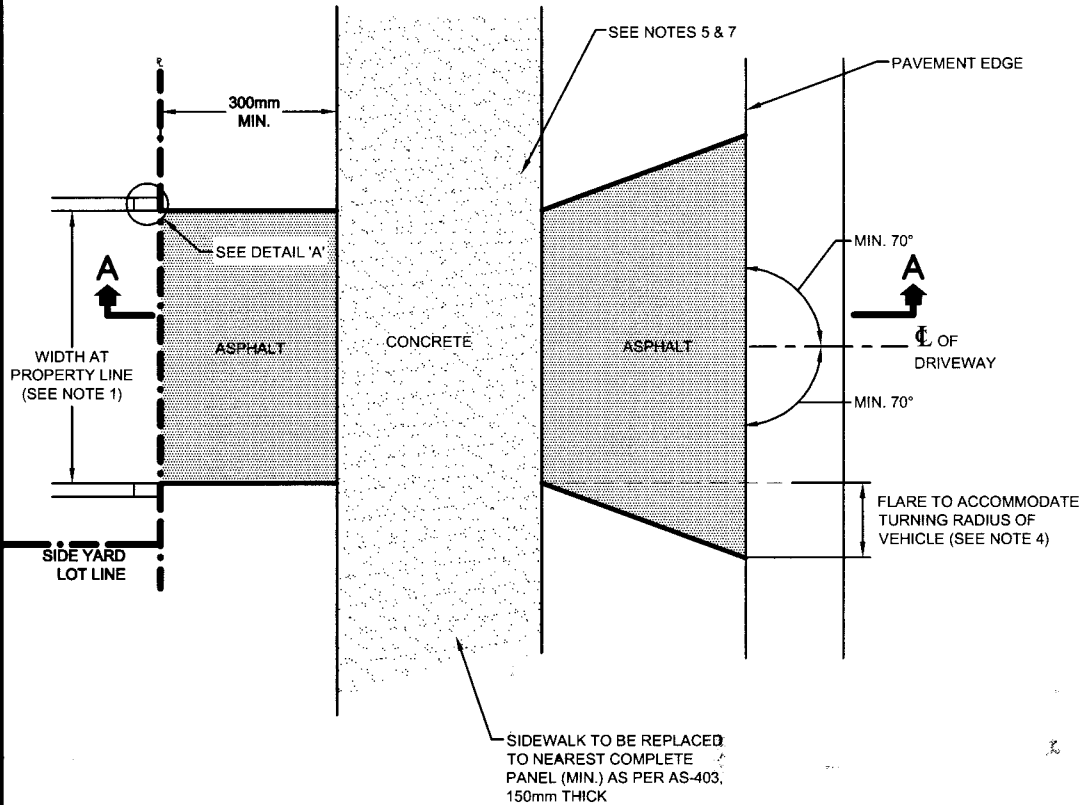
N/A



City Engineer or Designate

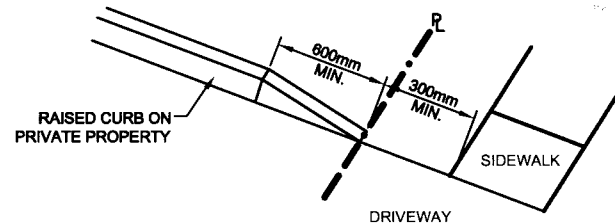
OCT 24/18
Date

Attachment – AS203

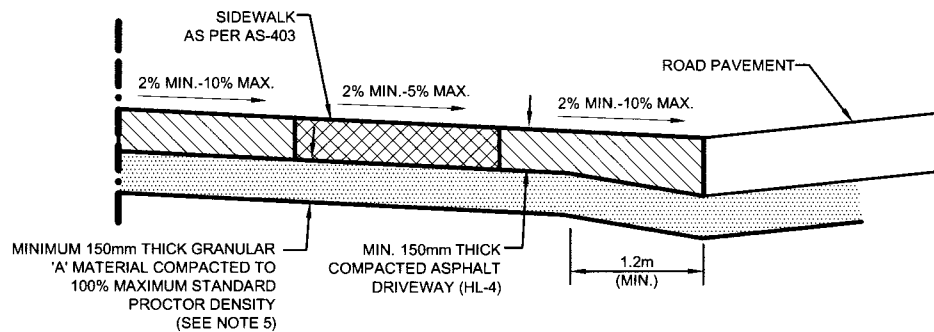


NOTES:


1. UNLESS OTHERWISE SPECIFICALLY APPROVED BY THE CITY ENGINEER, THE WIDTH OF COMMERCIAL DRIVEWAYS SHALL BE A MINIMUM OF 3.5 METRES PER LANE AND A MAXIMUM OF 4.5 METRES PER LANE, MEASURED AT THE PROPERTY LINE, AS PER APPLICABLE ZONING BY-LAW REQUIREMENTS.
2. UNLESS OTHERWISE SPECIFICALLY APPROVED BY THE CITY ENGINEER, RAISED CURBS BORDERING DRIVEWAY APPROACHES ON THE PUBLIC RIGHT-OF-WAY ARE PROHIBITED. ACCORDINGLY, ALL SUCH RAISED CURBS SHALL TERMINATE IN A MANNER SATISFACTORY TO THE CITY ENGINEER AS PER DETAIL A, BELOW, SO THAT THE RAISED PORTION OF THE CURB (OR THE ENTIRE CURB) SHALL TERMINATE AT THE PROPERTY LINE. IN ADDITION, THE MINIMUM CLEARANCE BETWEEN THE END OF THE RAISED PORTION OF THE CURB (OR THE ENTIRE CURB) AND THE NEAR EDGE OF THE SIDEWALK SHALL BE 300mm.
3. GRANULAR 'A' BASE COURSE SHALL BE PLACED TO A MINIMUM THICKNESS OF 150mm ON AN APPROVED SUBGRADE, COMPACTED TO 100% MAX. STANDARD PROCTOR DENSITY. WHERE UNSUITABLE SUBGRADE MATERIALS ARE ENCOUNTERED, THEY SHALL BE REMOVED AND REPLACED WITH COMPACTED GRANULAR 'A' MATERIAL TO THE SATISFACTION OF THE CITY ENGINEER.
4. DRIVEWAY MAY FLARE AT THE CURB TO ACCOMMODATE THE TURNING RADIUS OF COMMERCIAL VEHICLES, AS APPROVED BY THE CITY ENGINEER. THE FLARE SHALL OCCUR WITHIN THE APPROVED CURB CUT WIDTH IDENTIFIED ON THE PERMIT FROM ENGINEERING DEPARTMENT. FLARE NOT TO EXTEND BEYOND EXTENSION OF SIDE YARD LOT LINE.
5. WHERE AN ASPHALT DRIVEWAY ABUTS A CONCRETE SIDEWALK, IT SHALL DO SO IN A STRAIGHT (HORIZONTALLY AND VERTICALLY) MANNER, TO THE SATISFACTION OF THE CITY ENGINEER.
6. WHERE A CONCRETE SIDEWALK INTERSECTS A COMMERCIAL ASPHALT DRIVEWAY, THE SIDEWALK WILL BE EXTENDED THROUGH THE DRIVEWAY. WHERE THIS OCCURS, THE SIDEWALK SHALL BE 150mm THICK, IN ACCORDANCE WITH AS-403.
7. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY TO BE COMPLETED TO CITY OF WINDSOR STANDARDS, TO THE SATISFACTION OF THE CITY ENGINEER.



DETAIL 'A'
(SEE NOTE 2)



SECTION A-A

CITY OF WINDSOR			
ENGINEERING DEPARTMENT			
COMMERCIAL APPROACH - NO CURB			
DRAWN BY:	S.S., M.M.	DATE:	APRIL 2005
REVISION:	OCTOBER 2005	CHECKED BY:	W.B., S.S., P.U.
	FEBRUARY 2012; OCTOBER 2015		
CHECKED BY:	P.W., M.C.	PASSED BY:	M.W.
 CITY ENGINEER			AS-203